



draft

Small-Scale and Low-Rise

Residential Development Guidelines

for Mature Communities



Welcome

Fall 2020

- Learn about the draft guidelines
- Ask questions
- Complete a survey

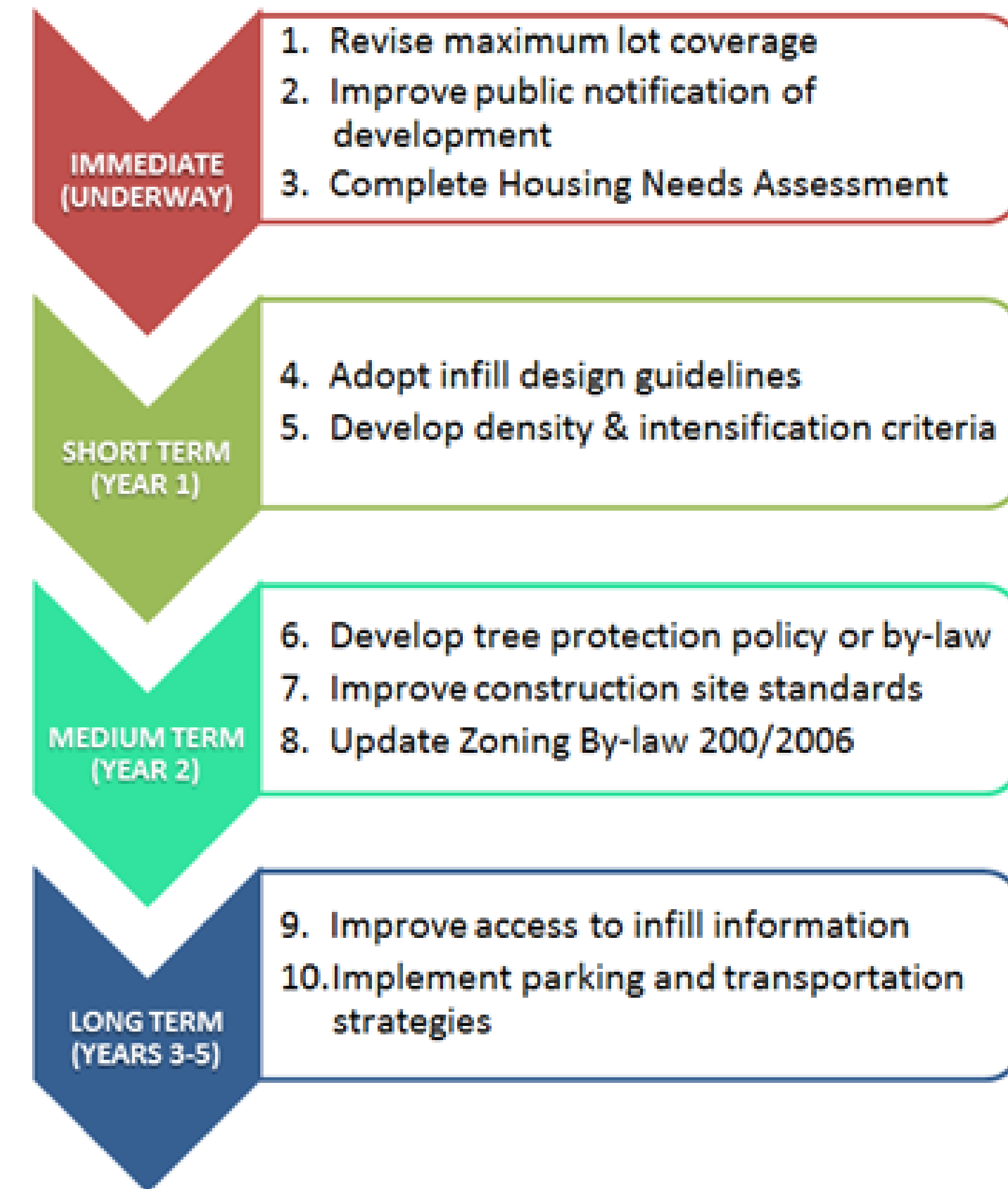


Scope of Project

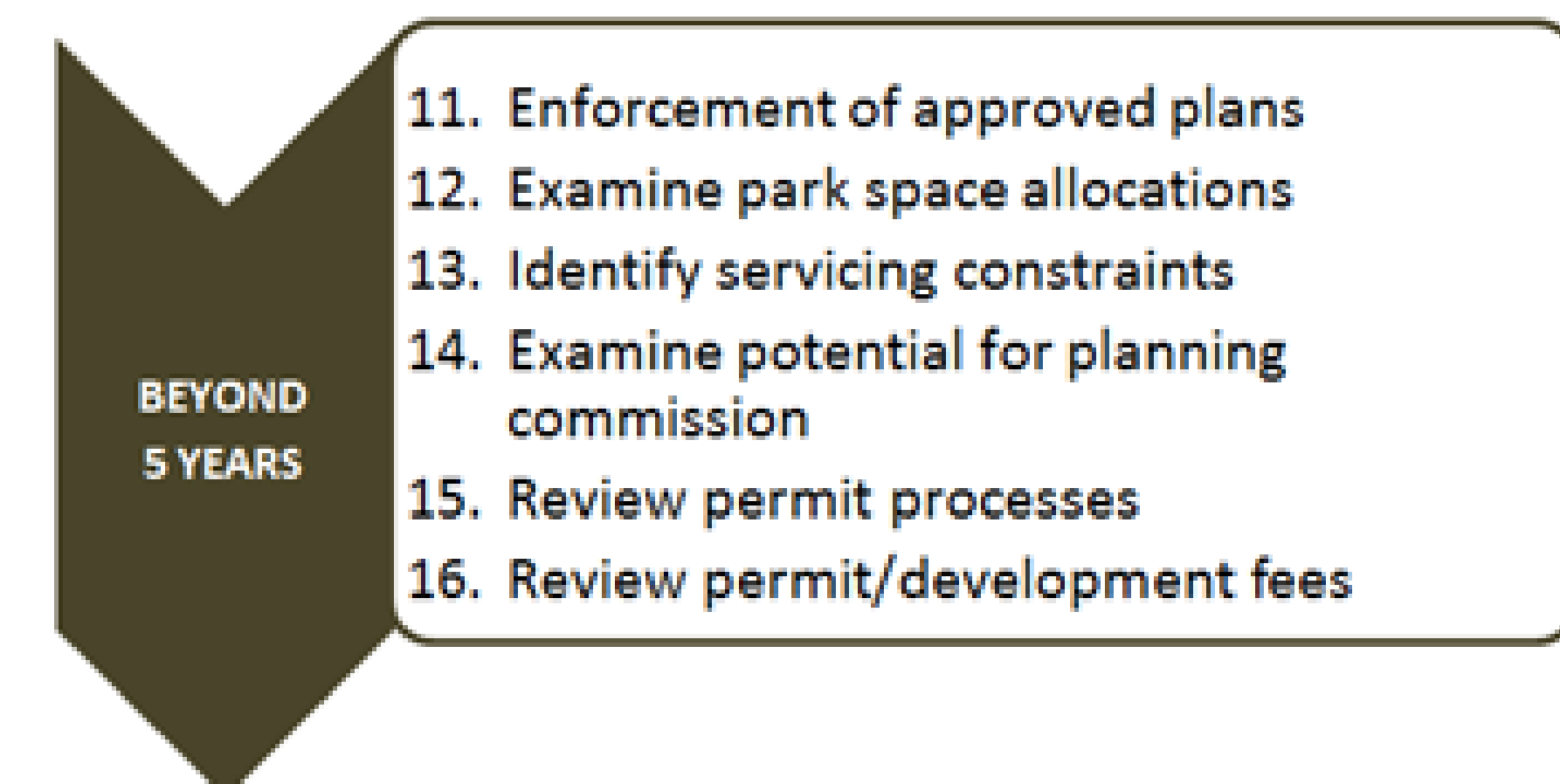
The development of the residential development guidelines focuses on the following 3 action items from the implementation action plan:

- Priority action 1: review **lot coverage**
- Priority action 4: infill **design guidelines**
- Priority action 5: develop **intensification criteria**

PRIORITY ACTIONS

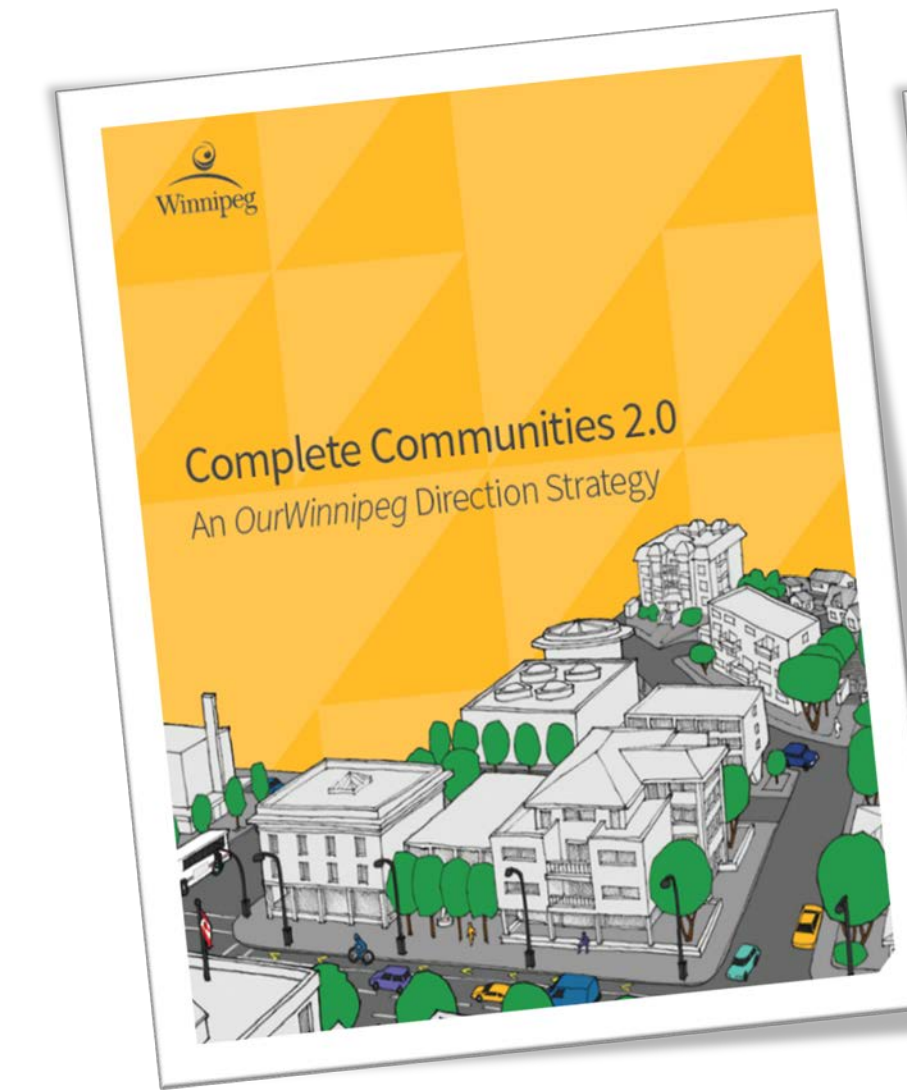


OTHER ACTIONS:

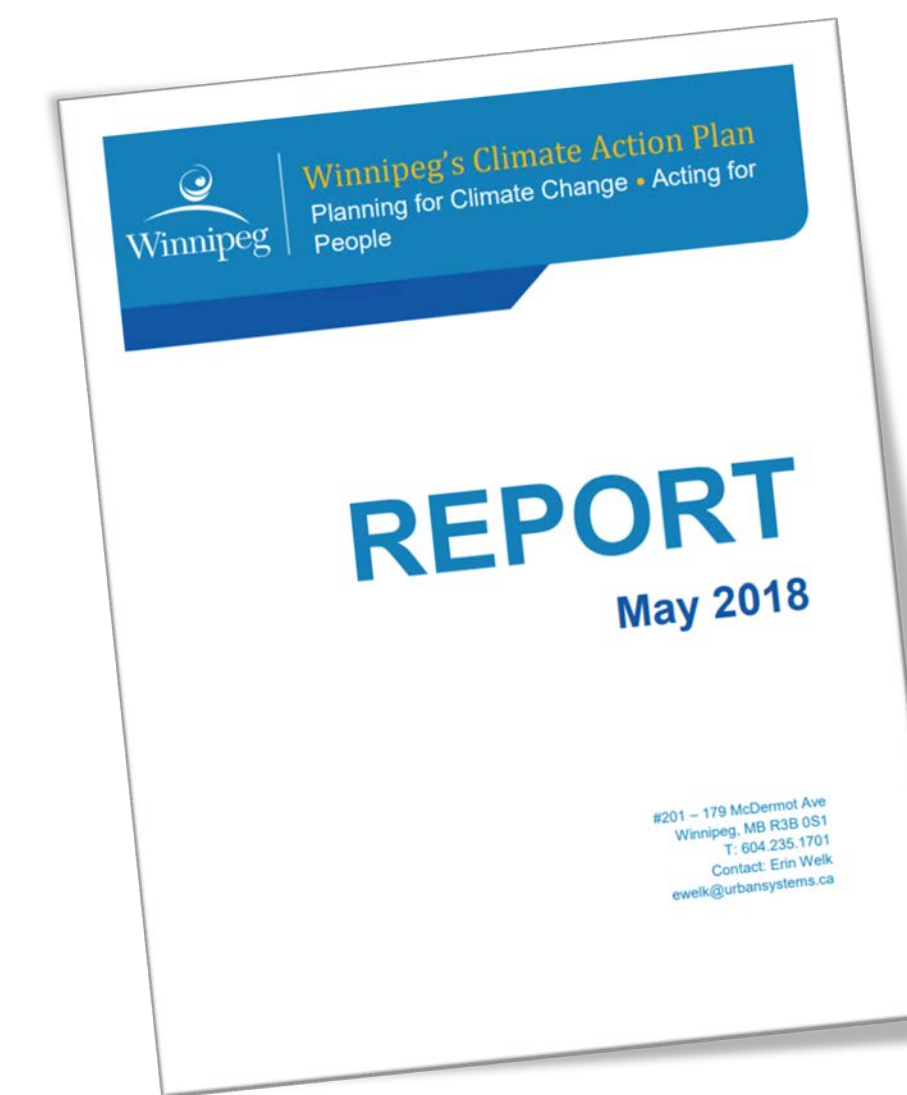


Policy alignment

- Encourage compatible residential development
 - Expand housing options in alignment with the intensification target
 - Create Infill Design Guidelines to direct location and design of development within Mature Communities
-
- Infill development is key to meeting greenhouse gas reductions
 - 50% target for growth to be located at strategic infill locations
 - Density should be located around Primary Transit Network
-
- Aligning density in proximity to the Primary Transit Network.



Draft
OurWinnipeg
& Complete
Communities 2.0



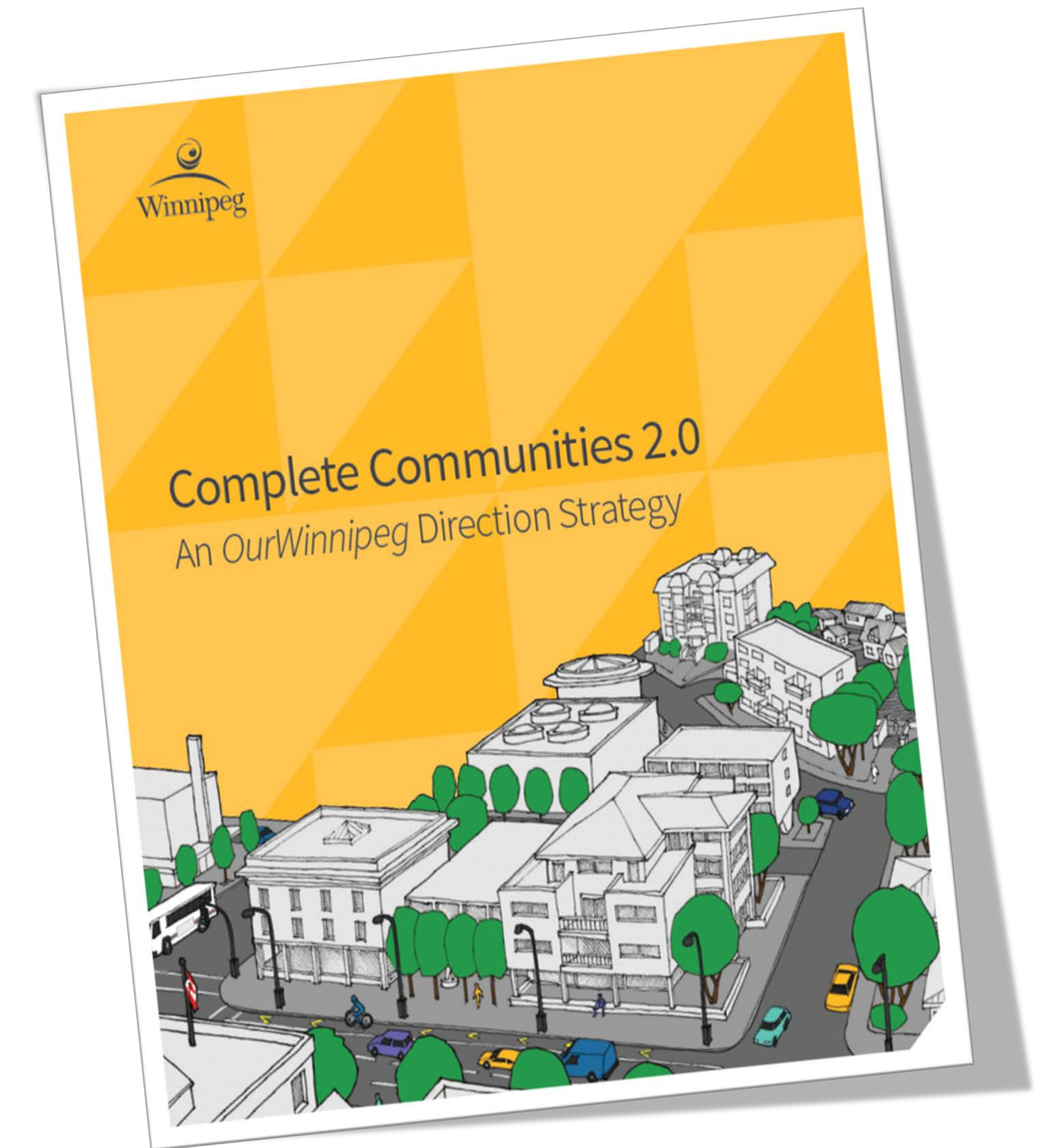
Winnipeg Climate
Action Plan



Draft Winnipeg Transit Master Plan

Policy Alignment

- Develop a residential infill strategy that will guide the location and design of residential infill development in established neighbourhoods.
- The creation of these guidelines should address the following goals:
 - Provide a diversity of housing options for all residents, at all life stages, in all neighbourhoods;
 - Maintain a balanced mix of housing within each neighbourhood;
 - Distribute additional residential density amongst mature neighbourhoods;
 - Contribute to the physical renewal and revitalization of older neighbourhoods;
 - Support transit and maximize walkability;
 - Make more efficient use of existing municipal infrastructure and community facilities;
 - Increases population levels to support retention of neighbourhood schools, commercial areas and main streets (i.e. Urban Mixed Use Corridors); and
 - Respects and enhances the character of existing neighbourhoods through compatible development.



Infill Strategy Project Goals

Why Infill Guidelines?

To address the many challenges associated with building in established areas to provide better outcomes that respect and enhance the character of existing neighbourhoods through compatible development.

Where will the guidelines and location criteria apply?

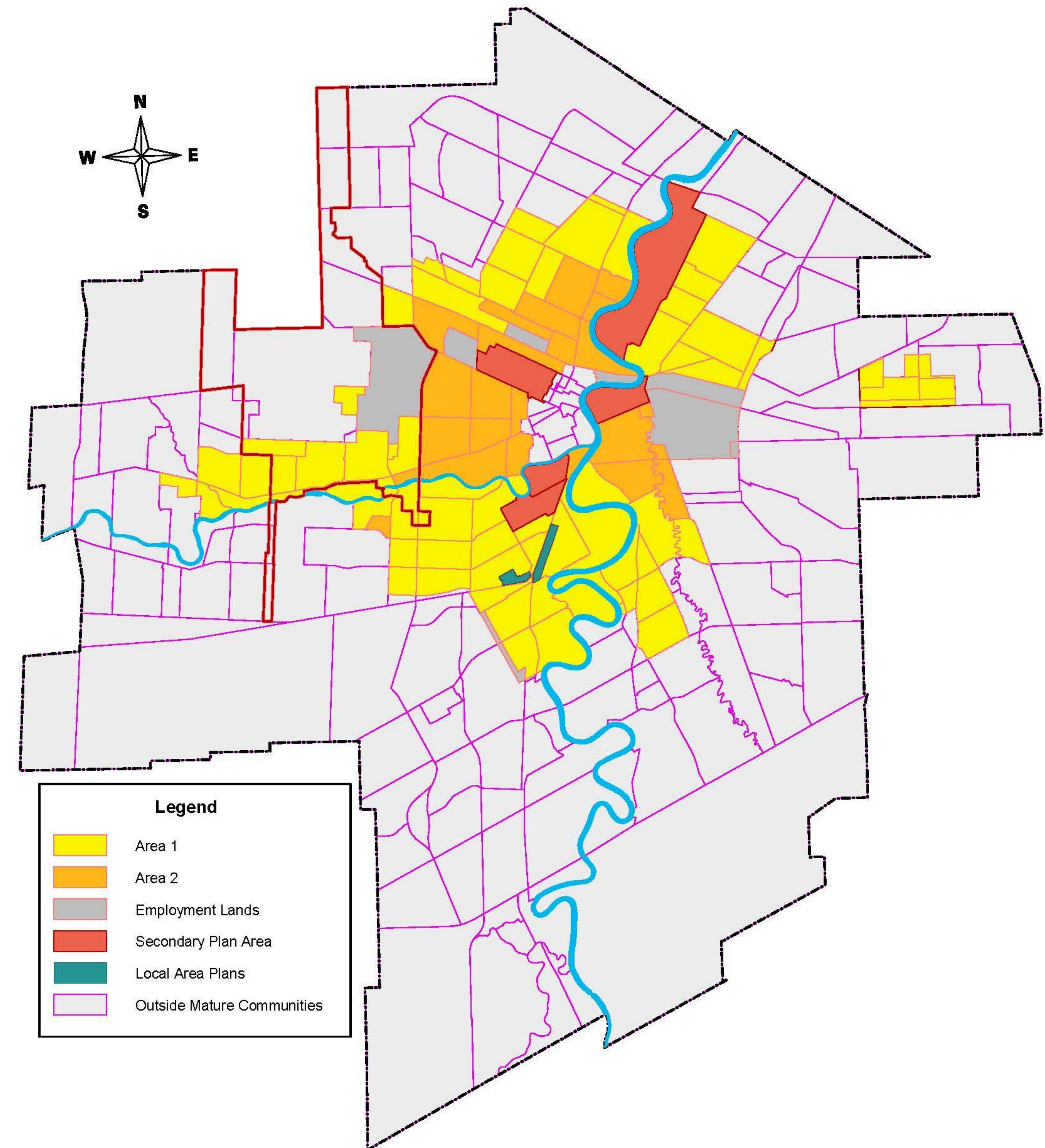
This project will establish guidelines for neighbourhoods identified as “Mature Communities” within the Complete Communities Direction Strategy. The recommended guidelines propose some variations based on where the proposed development is located, either within a low density neighbourhood or a mixed density neighbourhood, if it’s on an arterial street or a local street and what the context is in close proximity.

Where will the guidelines not apply?

Intent is for Guidelines to not supersede the policies that are covered by an existing local area plan or secondary plan or covered by another policy area within Complete Communities 2.0 (e.g. Downtown, Recent Communities, etc.)

What type of development will it apply to?

This project will apply to small scale infill development such as single family homes, duplexes/side-by-sides, three-unit dwellings, four-unit dwellings, townhouses and small scale apartments.



Consultation overview

- New buildings are often built too big for the context of abutting properties and the neighbourhood.
- Incentivize the preservation of existing mature trees and introducing/improving landscaping requirements for new builds.
- Multi-unit development should occur at corners, near transit services, commercial uses, or other multi-family residential buildings.
- Design elements that contribute to new housing being compatible with the established context are: building height, front yard setbacks, building façade materials, lot coverage and main entrance elevation.
- Greater clarification and predictability on what can be built and where is needed for builders and residents.

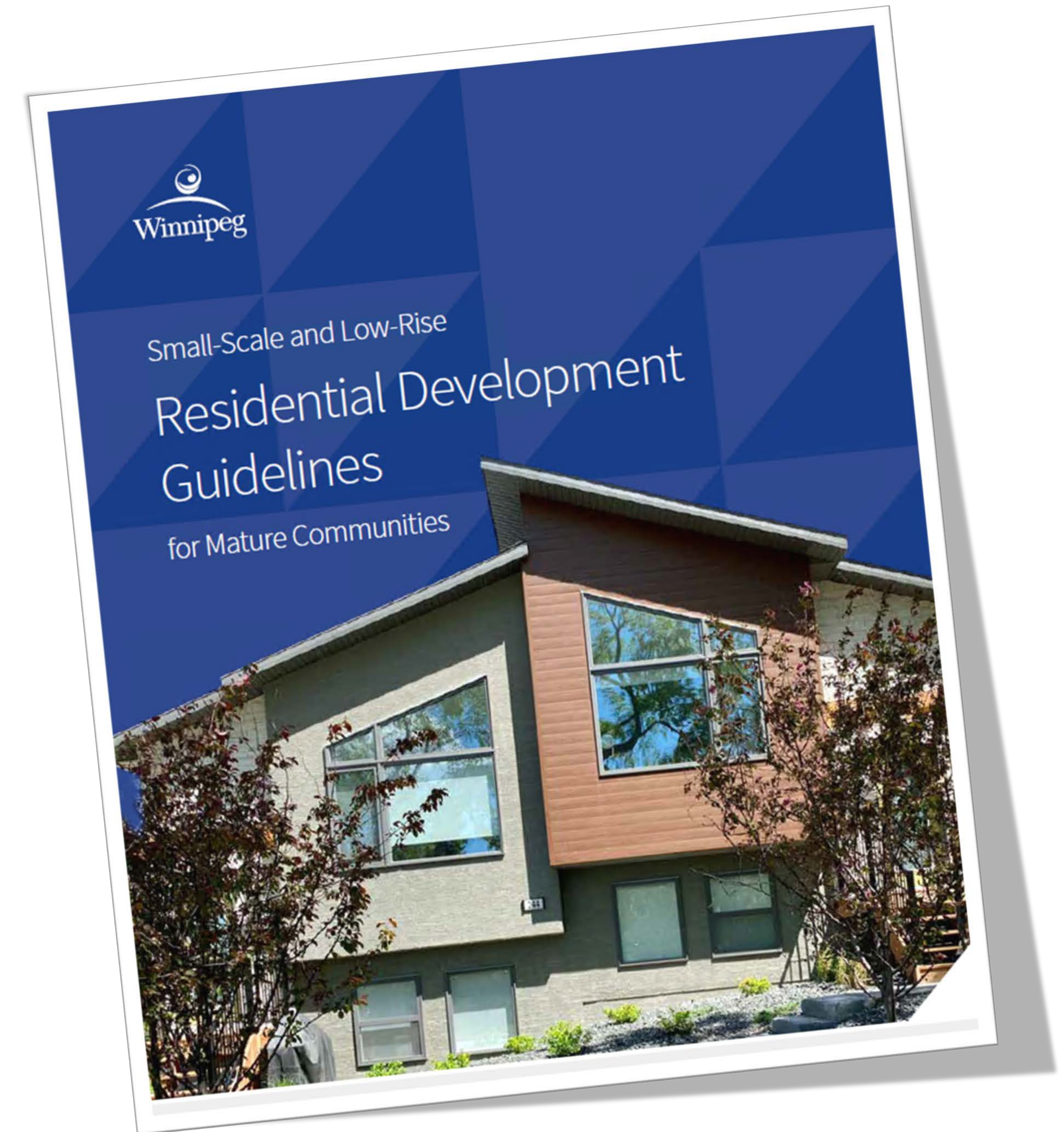
Consultation by the numbers:

Online Survey (2019):	2,000+
Open Houses (2019)	5
Design Workshops (2019):	2
External stakeholder meetings:	20+
Technical Advisory Committee meetings:	5



Project highlights: how will the Guidelines be used

- City staff will use the guidelines to provide design advice and review development proposals.
- Developers will use the guidelines to help guide how they design buildings.
- Zoning By-law will be updated to incorporate relevant elements of the Guidelines into regulations.



Project highlights: outcomes

- Single family homes will continue to be the predominant housing type for generations to come.
- Ensure new development contributes to the landscaping character of our neighbourhoods today while contributing to the mature tree canopy of tomorrow.
- Construction of various housing types is expected to occur incrementally over time and more evenly distributed through the city.
- Some of our older housing stock will be upgraded and fixed up to prolong their life however many houses that are in need of major repair/investment or do not meet today's housing needs will ultimately need to be replaced.
- New housing construction will meet today's construction standards, be free of lead and asbestos, and will be more energy efficient.
- New forms of small-scale housing will be distributed in neighbourhoods across the city to meet changing demographic needs and provide more housing to people at different stages (seniors looking to downsize or young adults looking for a starter home) in the neighbourhoods they want.
- As a whole, the Guidelines will help to mitigate the rising cost of housing, limit the size of new houses, provide greater housing choice, and prioritize a wide range of housing types for people of all ages, abilities and incomes.

Single-Family and Up-Down Duplex Design Guidelines

Key recommendations for single family dwellings:

- Building height maximums reduced to reflect more the context of the street
- Reduced building footprints (i.e. smaller lot area coverage)
- Introduction of landscaping requirements
- Introduction of building design criteria
- Changes to building setbacks (e.g. front yard, side yards and rear yards) to reflect more the context of the street



Single-Family and Up-Down Duplex

Based off of R1-M as typical Zoning Category	Current Regulation	Proposed Guidelines	Rationale
Lot Coverage	House - 40 % Garage 484 sq. ft. or 12.5% to max of 880 sq. ft.	<u>House with detached garage</u> <ul style="list-style-type: none">On lots less than 3,700 sq. ft. - 30% for house and 440 sq. ft. for the garage.On lots greater than 3,700 sq. ft. - 30% for house and 12% for the garage to a maximum of 880 sq. ft. <u>House with attached garage</u> <ul style="list-style-type: none">On lots less than 3,700 sq. ft. - 30% for house plus 440 sq. ft. for the garageOn lots greater than 3,700 sq. ft. – 30% for house and 12% for the garageAll living space above an attached garage will count toward lot area coverage.	<ul style="list-style-type: none">A reduced lot area coverage can address concerns associated with homes being “too big” while allowing for housing that is still viable for the market.A percentage gives flexibility for the size of dwelling and garage based on the size of lot, which will make homes more contextual.
Front Yard Setback	Minimum 15 ft. or average setback on the block	<ul style="list-style-type: none">Calculated based on the average setback of established dwellings on the block with the ability to move the building forward or back 5 feet from this average, but should not be less than 10 feet from the front property line.	<ul style="list-style-type: none">We heard that front yard setbacks of new dwellings should generally be consistent with the setbacks of the established dwellings.Allowing a 5 foot variability on front yard alignment respects the front yard context of the street while allowing greater flexibility in housing design and mitigating perceived privacy impacts on rear yards of abutting properties.
Rear Yard Setback	25 ft	<ul style="list-style-type: none">The minimum rear yard setback of the principal building shall be 36% of site depth or the average rear yard setback of abutting properties less 10 feet, whichever is less. In no case, shall the minimum rear setback be less than 25 feet.Where there is an attached garage, the rear yard will be assessed from the location of the living space of the dwelling.	<ul style="list-style-type: none">We heard that infill dwellings have been built to extend well into the rear yard, impacting enjoyment and livability of rear yard space for neighbours.Rear yard setbacks that are responsive to the context of abutting properties will allow for more contextually sensitive developments. This is particularly important on streets with large front yards.

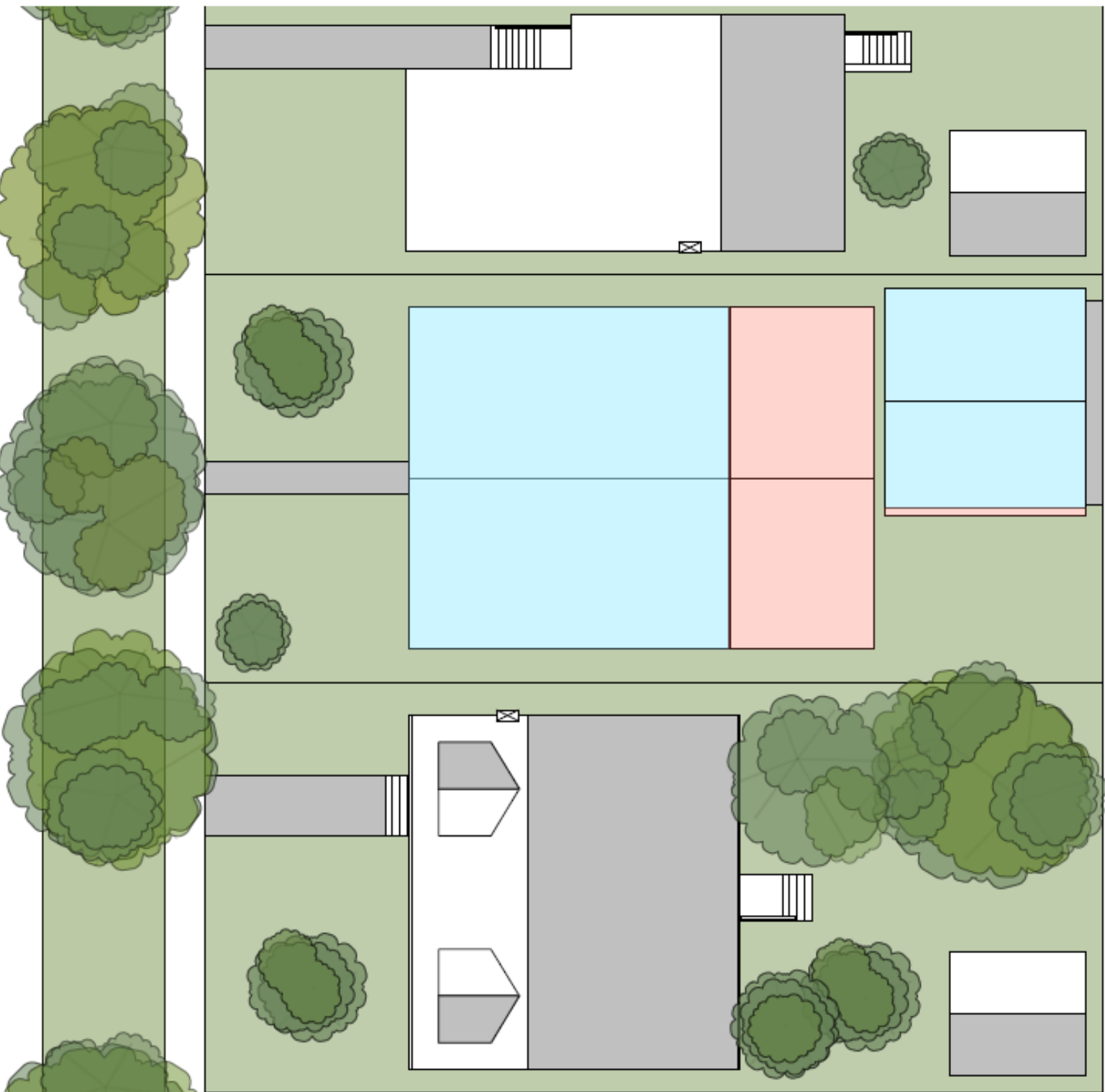


Figure 1 –The current lot coverage regulations are shown (in red) and proposed lot area guidelines (in blue) for house and garage for a 50 foot wide lot.

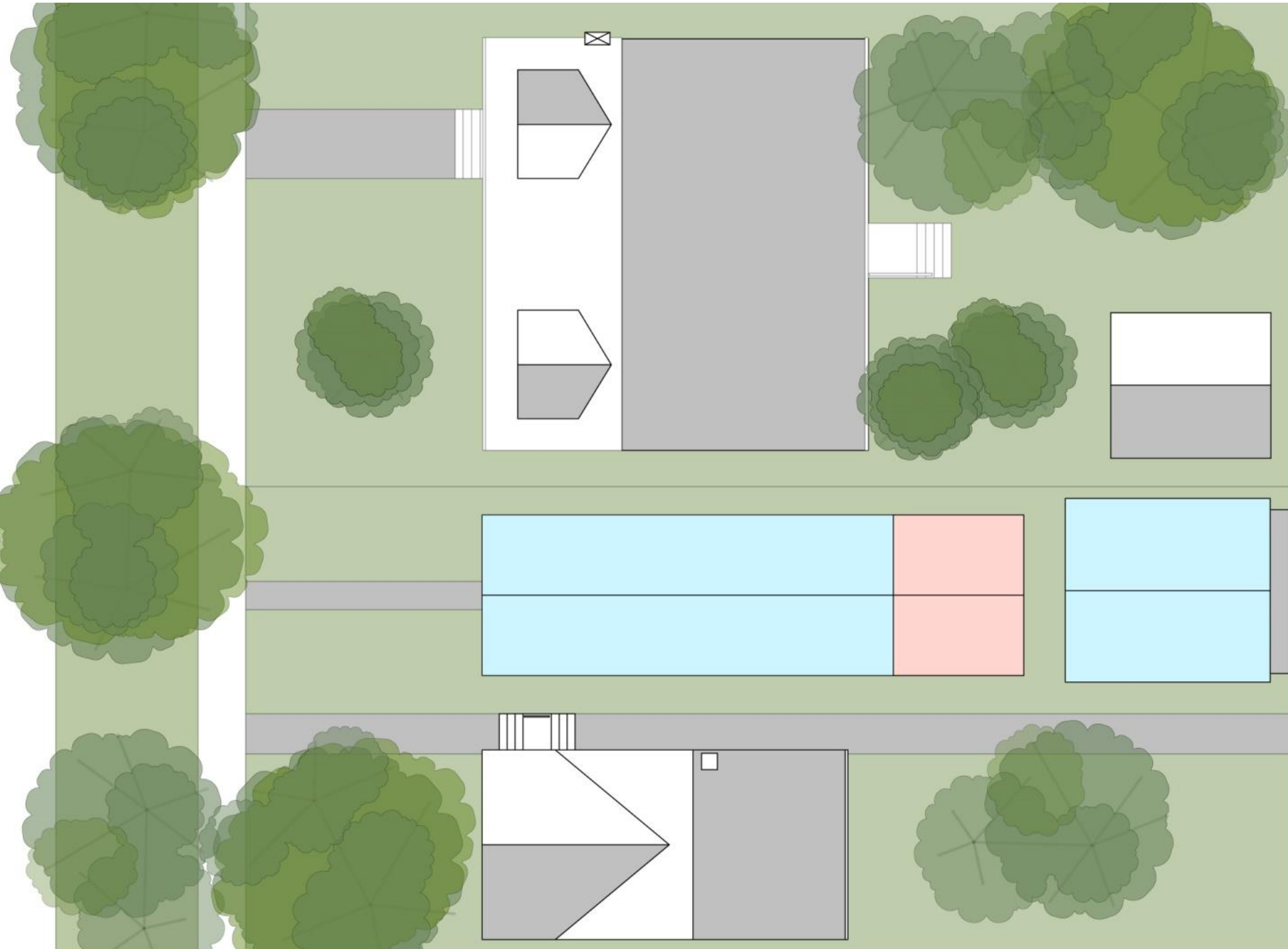


Figure 2 –The current lot area regulations (in red) and proposed lot area guidelines (in blue) for house and garage on a narrow lot example

Single-Family and Up-Down Duplex

Based off of R1-M as typical Zoning Category	Current Regulation	Proposed Guidelines	Rationale
Side Yard Setback	4 ft. on lots greater than 40 ft. wide 3 ft. on lots less than 40 ft. wide	<ul style="list-style-type: none"> Lots that are 30 feet wide or greater - 4 foot side yards on each side of the building; Lots less than 30 feet wide - 4 feet on one side and may have a minimum side yard of 3 feet on the other side. In all cases, one side of the dwelling shall have a set back of 4 feet and be clear of all projections on the main floor 	<ul style="list-style-type: none"> We heard that side yard setbacks that are consistent with the established character of the block was important so that new buildings don't feel overly imposing upon neighbouring properties. Proposed side yards guidelines will help to maintain a consistent rhythm and spacing between houses and address potential massing and privacy impacts on adjacent homes.
Projections into Side Yard	Max. depth is 2 ft. No closer than 1 ft. to property line; Maximum floor area is 20 sq. ft.	<ul style="list-style-type: none"> Cantilevered projections into the side yard shall be allowed for living space in the following manner for lots less than 30 feet wide: <ul style="list-style-type: none"> A one foot projection will be permitted on the side of the dwelling that is situated 3 feet from the property line. In the case of more than one projection, the aggregate total shall not exceed one third of the length of that house. The length of any one projection shall not exceed 10 feet. The projection will be limited to one storey; A two foot projection will be permitted on the upper storey of the side of the dwelling that is situated at 4 feet from the property line. In the case of more than one projection, the aggregate total shall not exceed one third of the length of that house side. The length of any one projection shall not exceed 10 feet; and 	<ul style="list-style-type: none"> Proposed projections will support the functional livability of narrow homes by allowing enough floor space in key rooms. Projections also add to the articulation and visual interest of buildings.

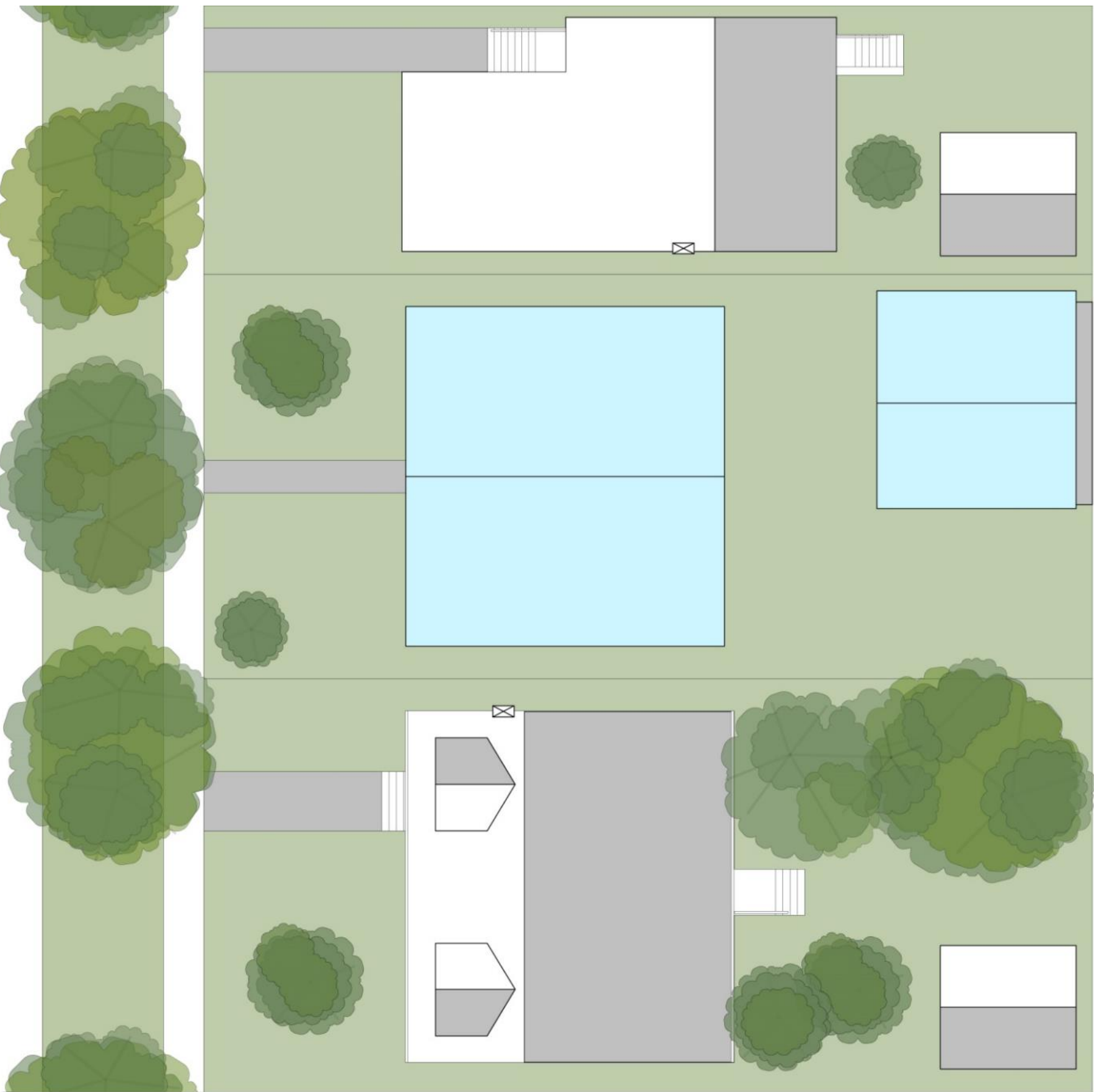


Figure 1 – 4 foot side yard setbacks on a 50 foot wide lot by 110 feet deep.

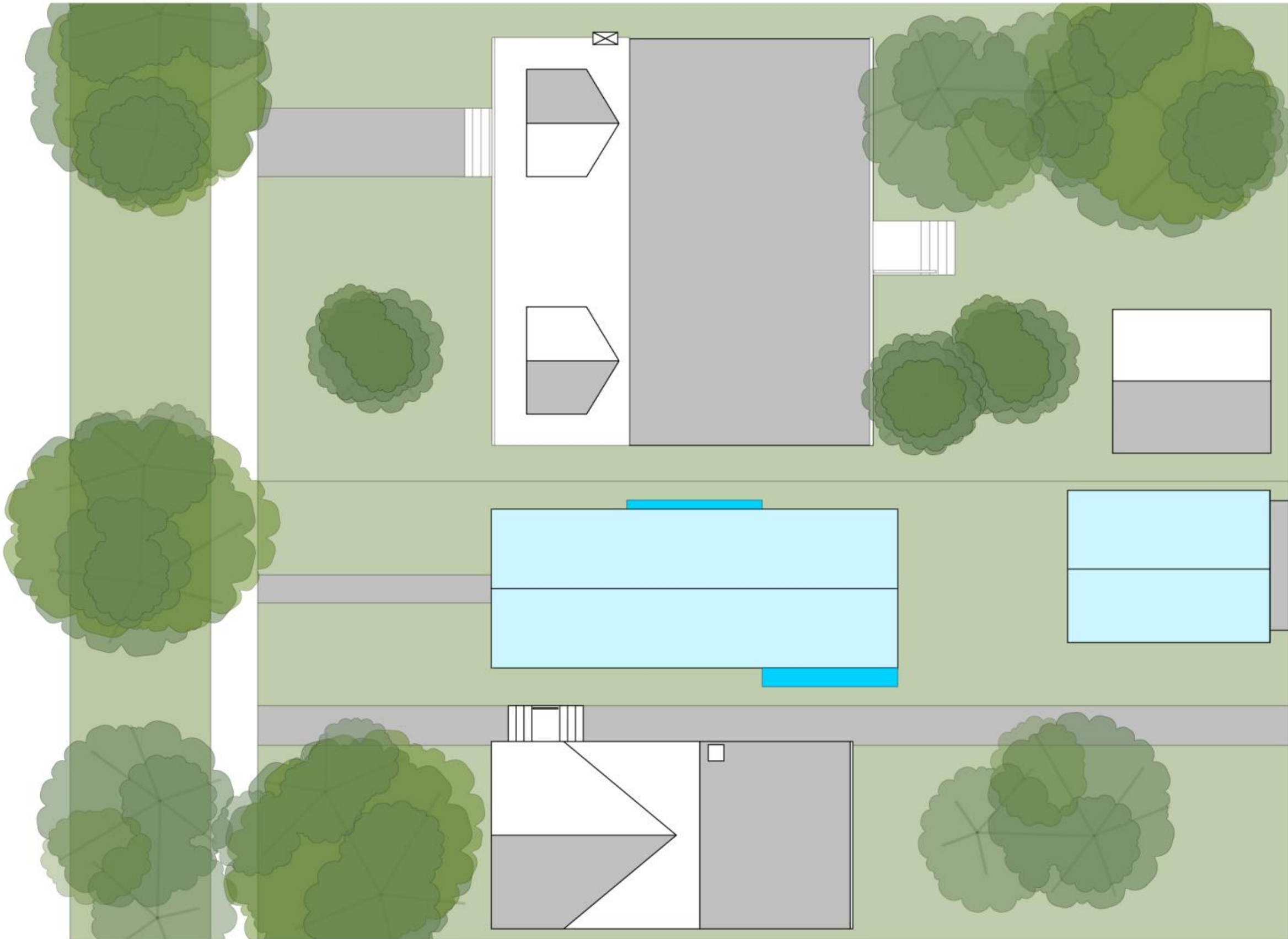


Figure 2 –Lot area coverage of a narrow lot dwelling with a 4 foot side yard and 3 foot side yards as well as permitted projections on a 25 foot by 110 foot deep lot.

Single-Family and Up-Down Duplex

R1-M is typical Zoning Category	Current Regulation	Proposed Guidelines	Rationale
Building Height	35 ft	<ul style="list-style-type: none"> For properties 60 ft. wide or less, the maximum building height shall be the greater of: <ul style="list-style-type: none"> (a) 28 ft.; or (b) the average of abutting properties plus 5 ft. to a maximum of 35 ft. For properties 60 feet or greater in width, the maximum building height shall be 35 feet. 	<ul style="list-style-type: none"> We heard that building heights of some new infill homes are perceived as being too tall in relation to some existing homes. Building heights that are responsive to the context of abutting properties and size of the lot will allow for more contextually sensitive developments.
Roof Pitches	No review or standards for roof pitch	<ul style="list-style-type: none"> The style of roof pitch should be considered in order to reduce a negative massing effect on adjacent properties. For example, the high point of a shanty roof should be directed toward the taller of the two adjacent buildings, or an alternative pitch design should be considered. 	<ul style="list-style-type: none"> Building height was noted as a significant issue and how a roof pitch is designed can affect building height and mass.
Main Entrance Elevation	No Regulation within the permitted buildable area	<ul style="list-style-type: none"> Should generally be similar to that of homes on the block in order to respect the established character of the street. Shall maintain a maximum finished floor height of 4 ft. above established grade. 	<ul style="list-style-type: none"> We heard that building height and visual appeal of infill homes was improved when the main floor height was lower. The proposed main entrance elevation requirement will help ensure infill development is sensitive in scale to adjacent properties and fit the character of the street.

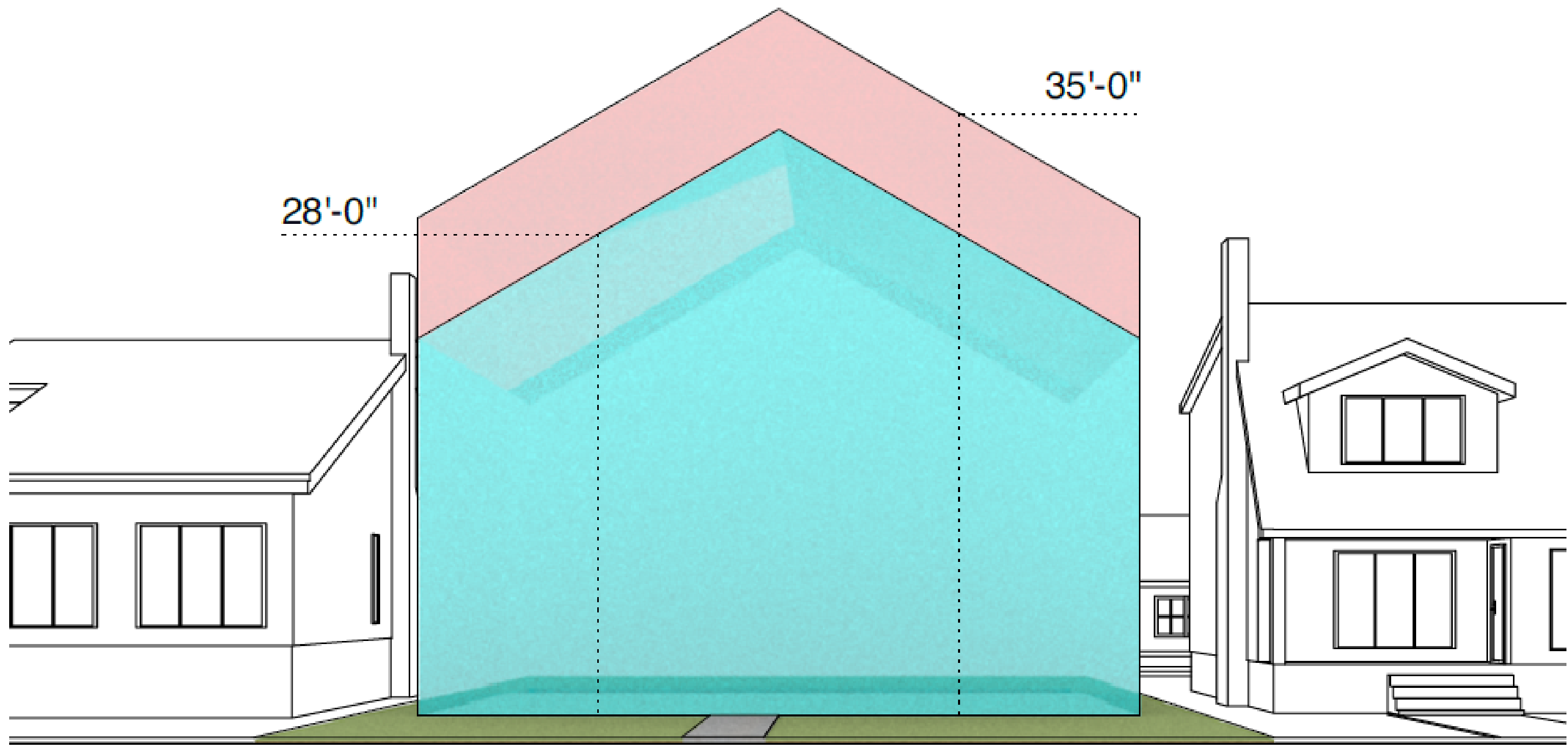


Figure 1 – Demonstrates current building height maximum (in red) and the proposed maximum (in blue)

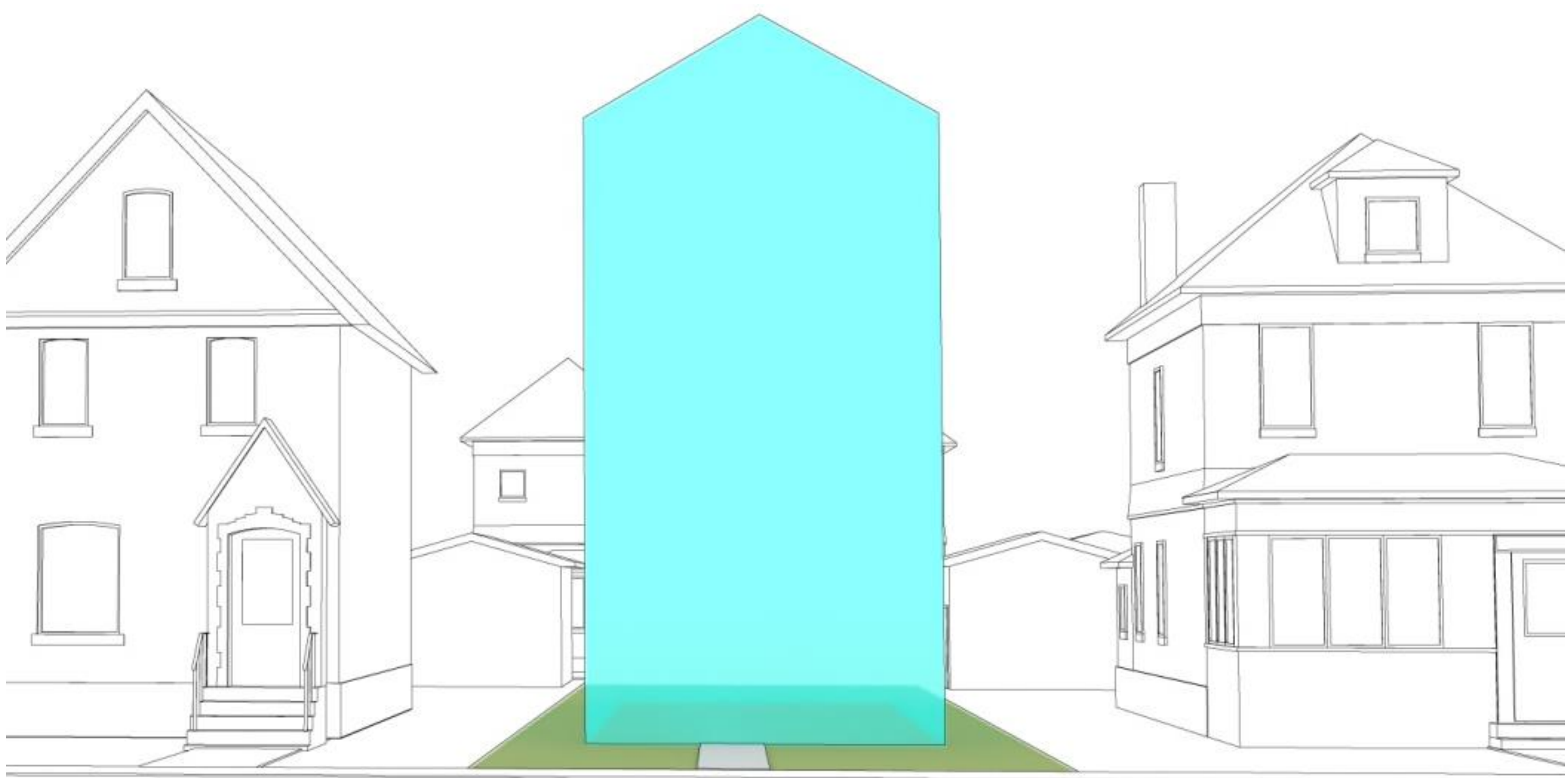


Figure 2 – Demonstrates maximum building height of 35 ft. by using the average of abutting properties plus 5 feet

Single-Family and Up-Down Duplex

R1-M is typical Zoning Category	Current Regulation	Proposed Guidelines	Rationale
Building Materials and Architectural Features	No Regulation	<p>Design considerations to include, but not limited to:</p> <ul style="list-style-type: none">• The design of homes shall include at least two primary durable building façade materials and at least two significant architectural features. Architectural features may include, but not limited to, varied roof lines, entry features (e.g. covered entries), bay windows, building articulations, dormers, shutters, window details, and porches.• Buildings are encouraged to incorporate architectural elements from the neighbourhood, such as porches, materials, roof lines, chimney details, window placements, and aligning floor heights, where possible.• Buildings on corner lots should extend architectural details and features, finishing materials and windows to the façade that faces the flanking street• Identical or mirrored front elevations shall not be located on abutting properties.	<ul style="list-style-type: none">• We heard that many participants had concerns about the quality, look and design of infill homes and how they “fit” into the neighbourhood context.• Adding design requirements for infill homes will improve the quality of infill housing while ensuring new homes integrate better into the established context of the street, without dictating architectural style.



Figure 1 – Demonstrates a mix of materials and articulation



Figure 2 – Demonstrates a variety of architectural features

Single-Family and Up-Down Duplex

R1-M is typical Zoning Category	Current Regulation	Proposed Guidelines	Rationale
Landscaping	No Regulation	<ul style="list-style-type: none"> For properties 35 feet or less in width, there should be a minimum of one tree (medium or large) and four shrubs. For properties greater than 35 feet in width and less than 50 feet in width, there should be a minimum of two trees (one medium or large and one small) and six shrubs. For properties greater than 50 feet in width, there should be a minimum of three trees (two large and one small) and six shrubs. An assessment of mature trees on the site should be provided and should describe how these trees will be retained where possible. Retained trees will count as credit toward landscape requirements. A minimum of 30% of the lot area should be reserved for soft landscaping. 	<ul style="list-style-type: none"> We heard that trees, shrubs and other natural features were significant character defining elements in a neighbourhood. Soft landscaping provides ground permeability, limiting stormwater runoff into sewers. Having incentives to keep healthy mature trees will help to protect our mature tree canopy and keep the character of our neighbourhoods.
Entrance Location	No Regulation	<ul style="list-style-type: none"> Main entrances shall be located on the front façade of the building, be clearly visible from the street and have a direct path to the public sidewalk. Side yard entrances should be secondary entrances and should be no higher than 2 ft. from grade in order to respect privacy between abutting properties. On a corner lot, secondary entrances should face the flanking street. 	<ul style="list-style-type: none"> We heard that entrance location and heights of entrances were closely associated with privacy to adjacent properties. Doors that are oriented to the street reflect the character of our mature neighbourhoods and positively contribute to street appearance.



Figure 1 – An example of the mature trees are characteristic of many mature neighbourhoods in Winnipeg



Figure 2 – Demonstrates an example of infill housing that was able to preserve existing mature trees on site as well as orient the secondary entrance to the flanking street

Single-Family and Up-Down Duplex

R1-M is typical Zoning Category	Current Regulation	Proposed Guidelines	Rationale
Driveway Access	No Regulation in Zoning By-law	<ul style="list-style-type: none"> Where a property abuts an improved lane, vehicular access will be required to be taken from the lane and no existing vehicular access from the roadway shall be permitted to continue . Any existing front approach must be removed and that the curb, sidewalk and boulevard be restored. Where a property does not abut a lane, the driveway access should not have a negative impact on mature trees within the public right-of-way, and its design should minimize conflict between vehicles and non-motorists and offer opportunity for shared driveway use. The width of a driveway shall reflect the context of the block but in no case should the width of the driveway exceed 10 feet for a single car garage and 20 feet for a two-car garage or greater. 	<ul style="list-style-type: none"> Maintaining or re-establishing an uninterrupted sidewalk improves the pedestrian environment and safety by minimizing conflict with vehicles. Reducing the width of driveways allows for more on-street parking, more area for landscaped front yards and minimizes conflict with pedestrians.
Privacy	No Regulation	<ul style="list-style-type: none"> Where exterior living spaces, such as decks or balconies are provided measures are being recommended to mitigate privacy concerns. This may be achieved through, but is not limited to, opaque fencing, placement of windows offset from neighbouring property, recessing of balconies. 	<ul style="list-style-type: none"> We heard that privacy and sightlines were identified as an issue that could be improved upon with new infill development.

Figure 1 – Demonstrates how mature boulevard trees are preserved when new housing takes access from a public lane



Two-Unit Side-by-Side, Three- and Four-Unit Design Guidelines

Key recommendations for two, three and four unit dwellings:

- The previous recommendations for single family and up/down duplex will also apply to the two-family side-by-side, three and four unit housing typologies.
- Additional considerations include:
 - Lot area coverage
 - Entrance location requirements
 - Mechanical and utility locations and screening



Two-Unit Side-by-Side Dwellings

All guidelines proposed for single family dwellings will apply to two-unit dwellings with the addition of the following:

R2 is typical zoning district	Current Regulation	Proposed Guidelines	Rationale
Side Yards	4 foot side yards	<ul style="list-style-type: none">The minimum required side yard setback shall remain at 4 feet	<ul style="list-style-type: none">This maintains a form that is similar to what would be permitted for a single family dwelling.
Entrance Location	No regulation	<ul style="list-style-type: none">Main entrances for each principal unit shall be located on the front façade of the building, be clearly visible from the street and have a direct path to the public sidewalk. In the case of a corner lot, one of the main entrances is encouraged to be located facing the flanking street.	<ul style="list-style-type: none">To ensure that main entrances are visible and the design of the building positively contributes to the pedestrian experience.

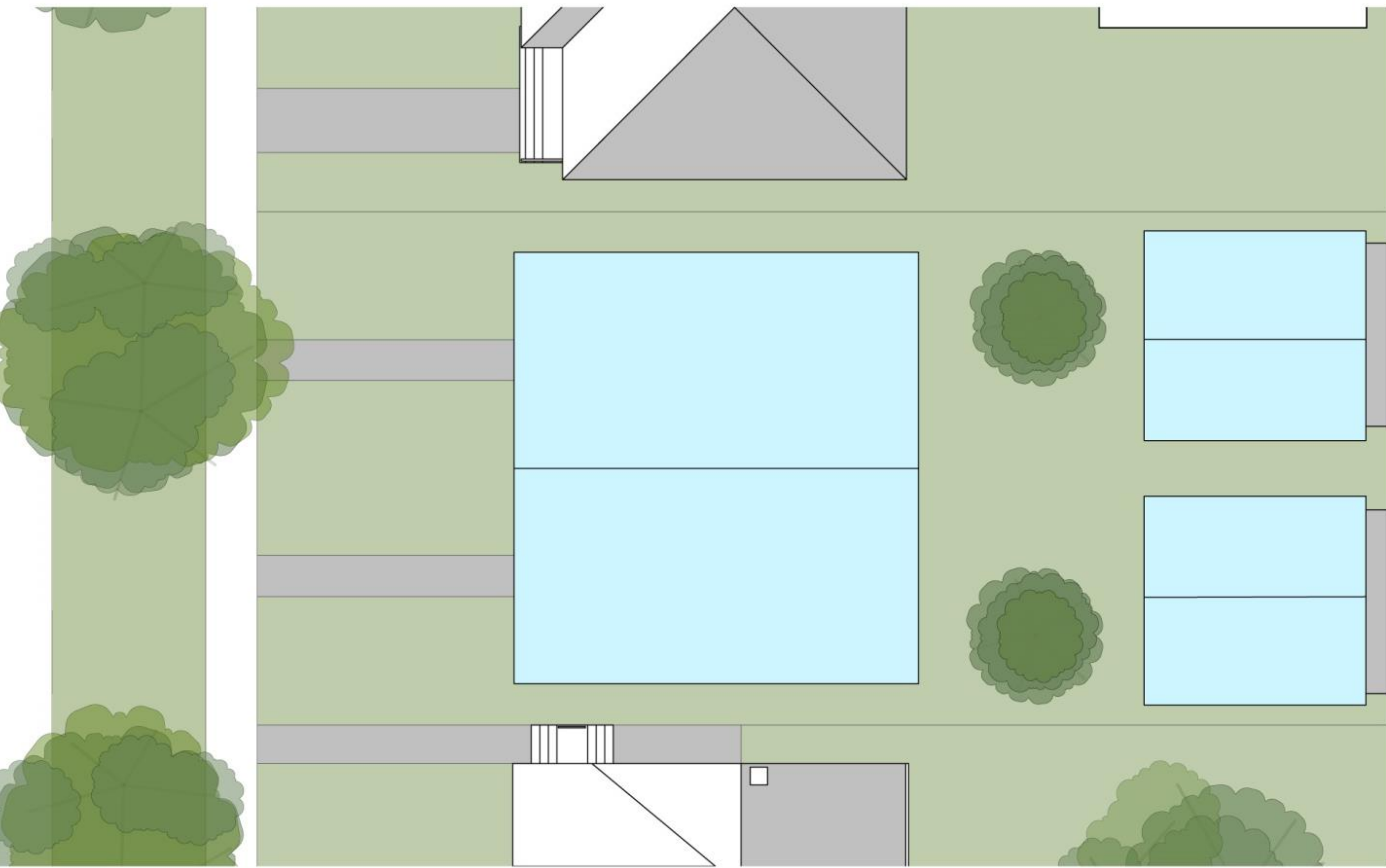


Figure 1 – demonstrates what a side-by-side dwelling could look like with 30 percent lot coverage on a 50 foot wide lot and 440 sq. ft. garages for each unit.

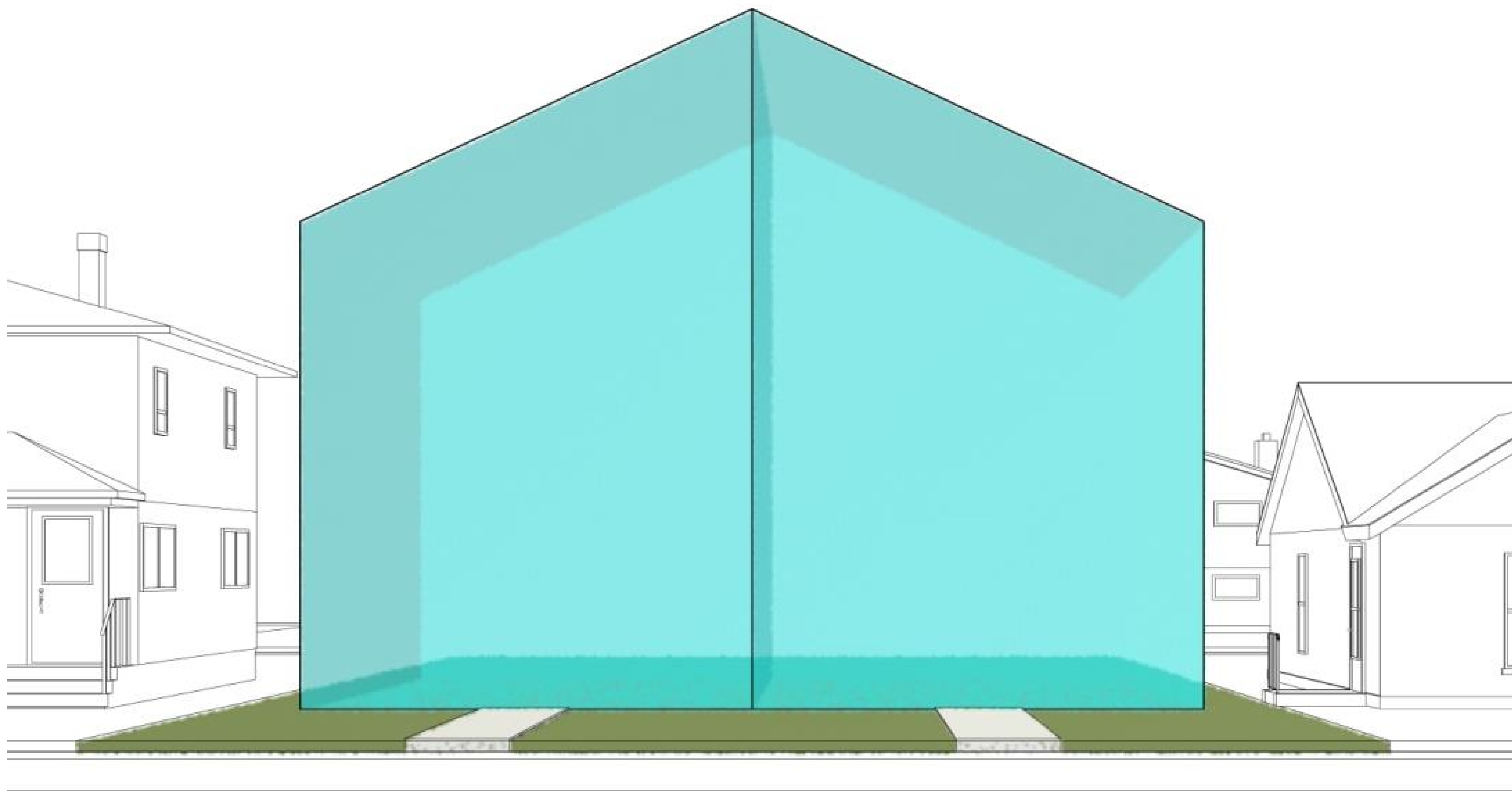


Figure 2 – demonstrates what a side-by-side dwelling could look like with 30 percent lot coverage on a 50 foot wide lot and 440 sq. ft. garages for each unit.

Three-Unit Dwellings

All guidelines proposed for single family dwellings will apply to three unit dwellings with the addition of the following:

R2 is typical zoning district	Current Regulation	Proposed Guidelines	Rationale
Side Yards	4 foot side yards	<ul style="list-style-type: none">Up-down three-unit buildings and side-by-side three-unit buildings shall maintain a minimum standard side yard setback of 4 feet from the property line.	<ul style="list-style-type: none">This maintains a form that is similar to what would be permitted for a single family dwelling.
Entrance Location	No current regulation	<ul style="list-style-type: none">Three-unit buildings that contain up-down configurations may provide individual entrances for the ground floor units and/or a shared building entrance.Main entrances for each principal unit/and or a shared building entrance shall be located on the front façade of the building, be clearly visible from the street and have a direct path to the public sidewalk.	<ul style="list-style-type: none">To guide development toward having clearly identifiable and accessible entrance locations. This will also ensure the design of the building positively contributes to the pedestrian experience.
Utilities, Servicing and Mechanical	No current regulation	<ul style="list-style-type: none">Exterior-facing doors for the purposes of a common water metre area will be discouraged to locate along the façade of the building facing the street.	<ul style="list-style-type: none">This recommendation is meant to encourage a design solution to address providing water meters in an accessible manner, while maintaining a well designed street facing façade.

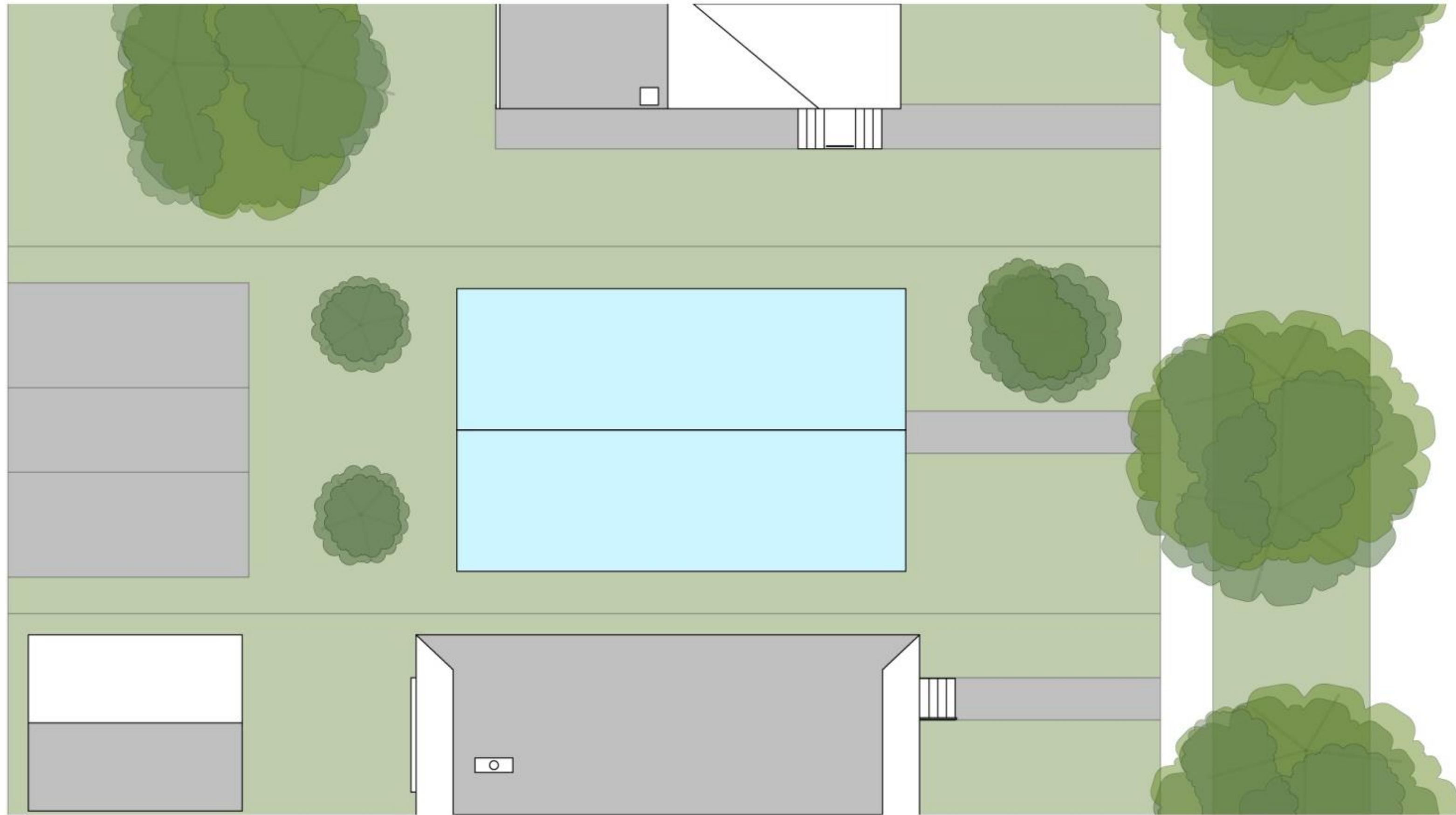


Figure 1 – Three unit dwelling with 30 percent lot coverage on a 35 foot wide lot

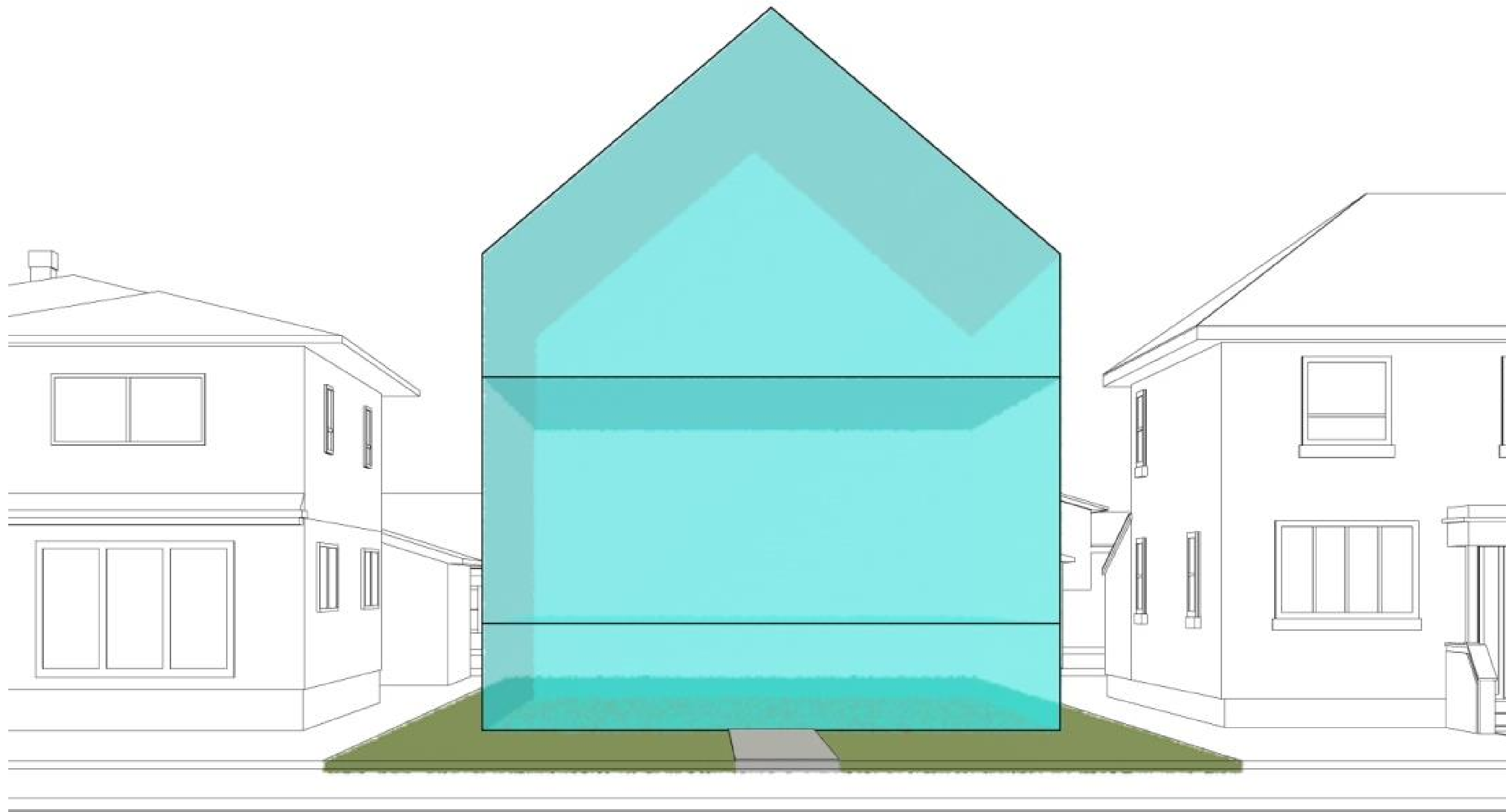


Figure 2 – Example of an up/down three-unit building in context with proposed dimensional standards

Four-Unit Dwellings

All guidelines proposed for single family dwellings will apply to four unit dwellings with the addition of the following:

R2 is typical zoning district	Current Regulation	Proposed Guidelines	Rationale
Side Yards	4 foot side yards	<ul style="list-style-type: none">Maintain a minimum standard side yard setback of 4 feet on both sides.	<ul style="list-style-type: none">This maintains a form that is similar to what would be permitted for a single family dwelling.
Lot Coverage	No maximum	<ul style="list-style-type: none">The maximum lot coverage for the principal dwelling shall be 30%. Lot coverage for the principal building may be increased up to 42% by taking the lot coverage allocation from the garage lot coverage.The maximum lot coverage for detached garages shall be 12%.The maximum lot coverage for principal buildings with attached garages shall be 42%.All living space above an attached garage will be counted as part of the lot area calculation for the principal dwelling.Unenclosed porches and decks do not count toward the lot area coverage.	<ul style="list-style-type: none">Four unit dwellings may require more lot coverage than a three unit dwellings because each unit may not be able to be built on top of the other. As such, we have provided the option for builders of four-unit dwellings to re-allocate lot coverage from a garage to the principal building.
Entrance Location	No regulation	<ul style="list-style-type: none">Up-down configurations may provide individual entrances for the ground floor units and/or a shared building entrance.Four-unit buildings with side-by-side configurations shall incorporate individual entrances on the front façade of the building, be clearly visible from the street and have a direct path to the public sidewalk.	<ul style="list-style-type: none">To guide development toward having clearly identifiable and accessible entrance locations. This will also ensure the design of the building positively contributes to the pedestrian experience.

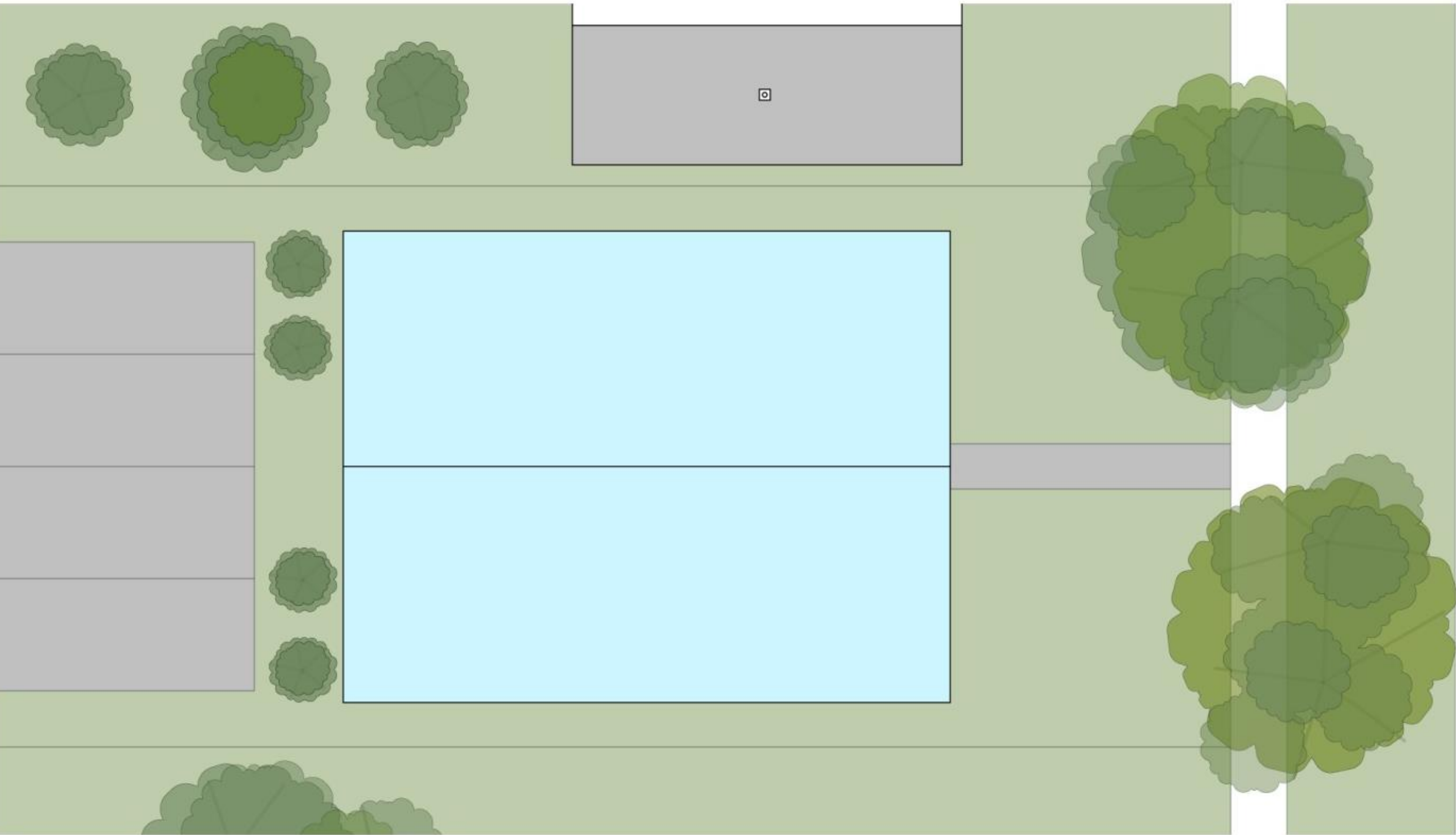


Figure 1 – a four unit dwelling with the 42 % coverage and surface parking

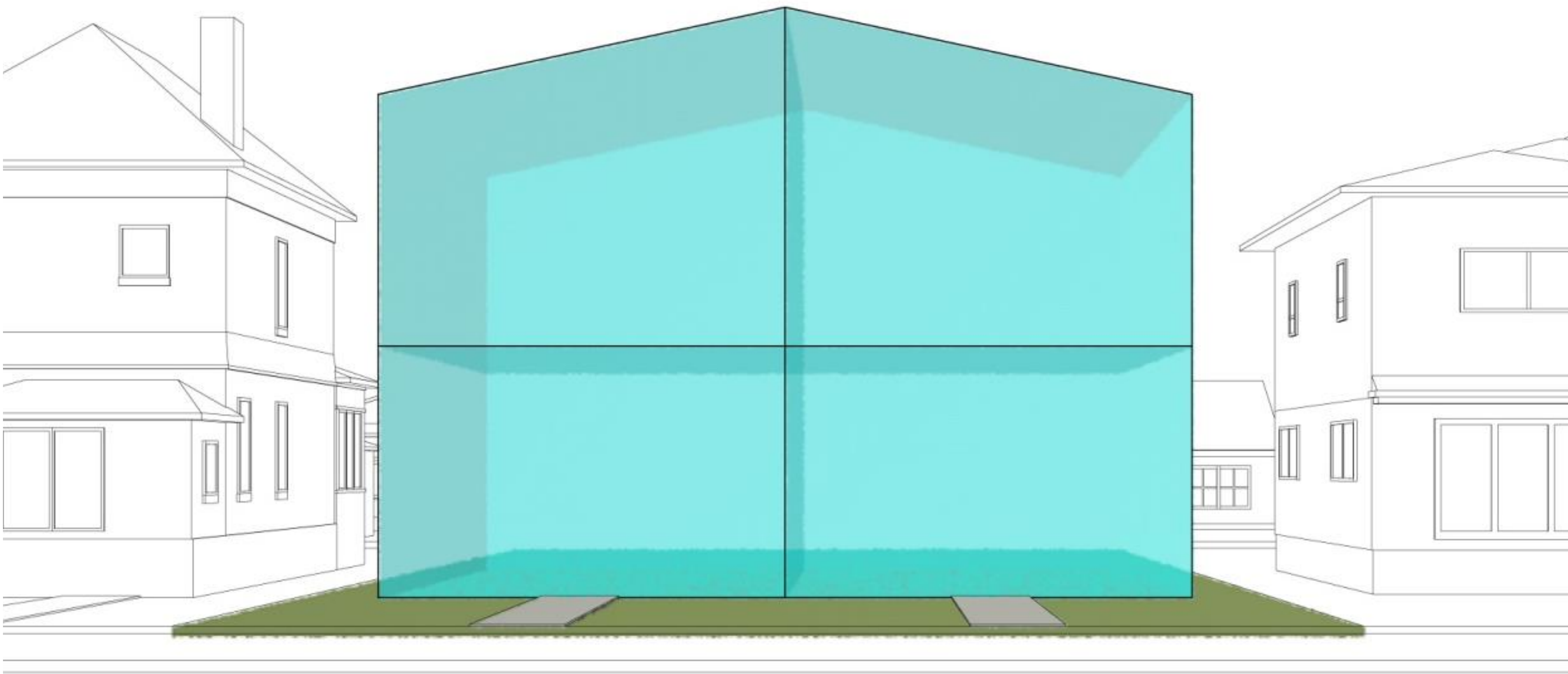


Figure 2 – Example of four-unit building in context with proposed dimensional standards

Low-Rise Apartments and Townhomes

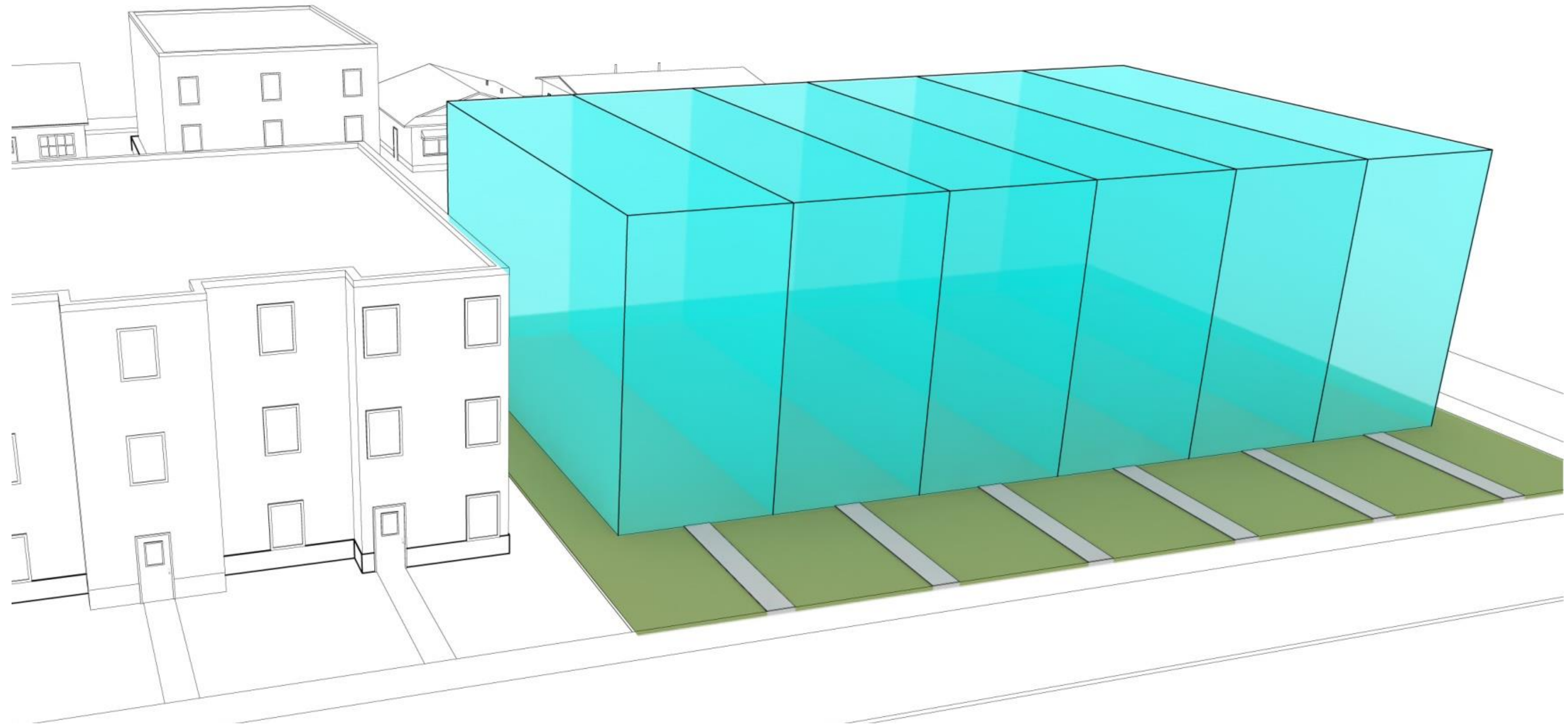
Key recommendations for low-rise apartment buildings and townhomes:

- Recommended maximum lot area coverage
- Changes to landscaping requirements
- Introduction of building design criteria
- Changes to required yard setbacks

Recommendations respond to location.

For example:

- Different building height limits will apply based on the type of street the building is to be located.
- Building setback requirements change based on type of street and how tall the building is proposed to be.



Low-Rise Apartments and Townhomes

RMF-S is typical Zoning Category	Current Regulation	Proposed Guidelines	Rationale
Rear Yard Setback	25 foot rear yard setback	<ul style="list-style-type: none"> Rear yard setbacks should be contextually sensitive to adjacent properties and where possible be consistent with the pattern of the block. <ul style="list-style-type: none"> On an arterial street, the minimum rear yard setback should be 25% of the lot depth or the average rear yard setback of abutting properties less 10 feet, whichever is less. In no case, shall the minimum rear setback be less than 25 feet. For properties located on a local or collector street, the minimum rear yard setback should be 30% of lot depth or the average rear yard setback of abutting properties less 10 feet, whichever is less. In no case, shall the minimum rear setback be less than 25 feet. Where no lane is present and the proposed building abuts a single or two unit dwellings, buildings should be located within a 45 degree angular plane measured from the rear property line (starting at a six foot height above grade). 	<ul style="list-style-type: none"> This approach is more responsive to various lot dimensions and depths and is responsive to immediate context. The 45 degree angular plane helps minimize building overlook and shadows onto adjacent properties.
Side Yard Setback	4 foot side yard setbacks	<ul style="list-style-type: none"> Where the property is located on a local or collector street and shares a property line with a park use, or a single- or two-family use on a local or collector street, the minimum side yard shall be determined on the following basis: <ul style="list-style-type: none"> A minimum of 6 feet for any portion of the building 28 feet tall or less. For any portion of the building that is over 28 feet, the massing shall setback 1 foot for every additional increase in height of 1 foot. Where the property is located on an arterial street or shares a property line with a non-residential use or multi-unit residential use, the minimum side yard shall be determined on the following basis: <ul style="list-style-type: none"> A minimum of 6 feet for a building that is 35 feet or less in height, or; 10 feet for a building that is 35 feet or greater up to a maximum of 45 feet. 	<ul style="list-style-type: none"> Side yard setbacks are proposed in a manner that responds to the street type the building is to be located on or the context of adjacent properties. Building setbacks are proposed to reduce building massing impacts as the building gets taller.

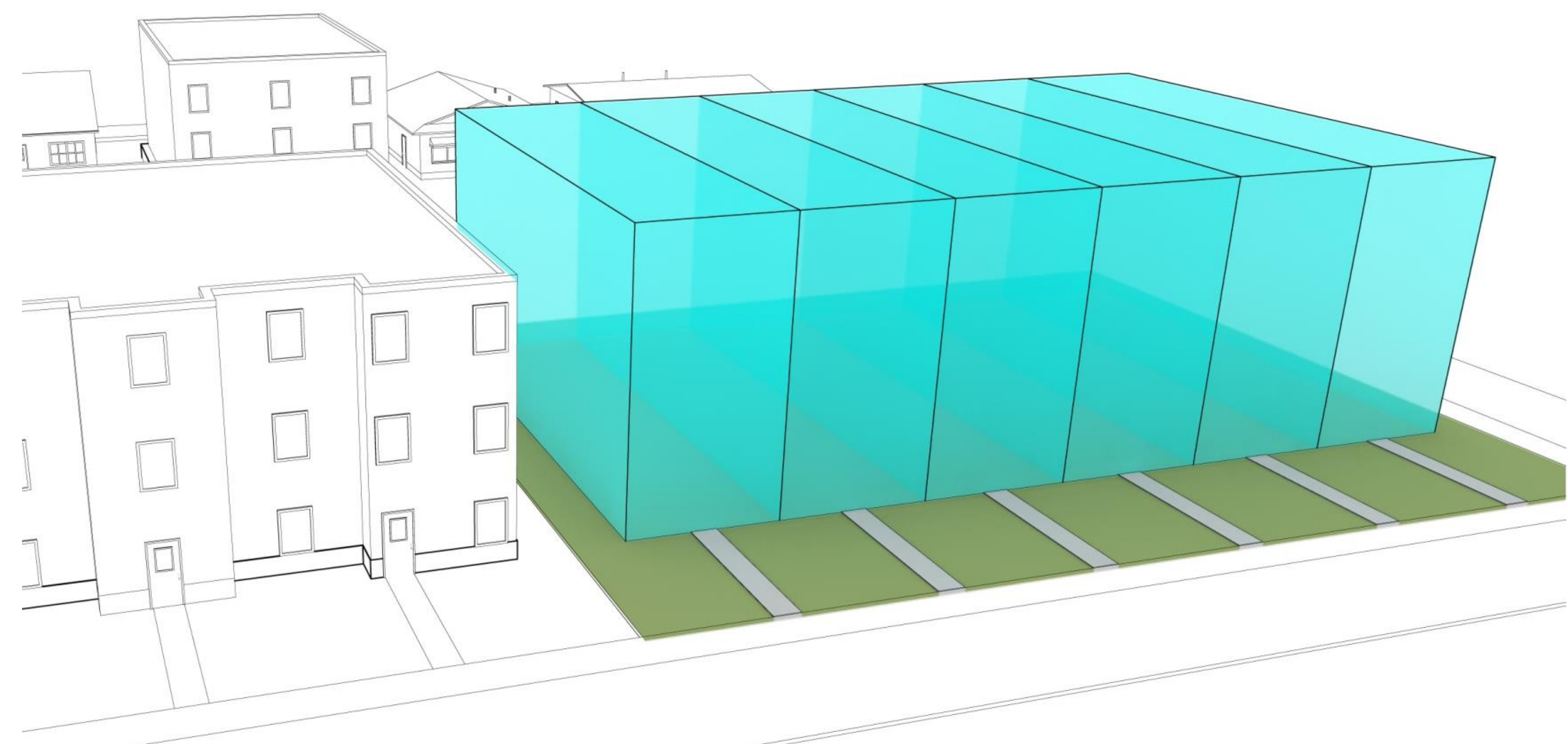


Figure 1 – demonstrates a townhouse dwelling on a collector street with 6 foot side yards and a 35 foot tall building

Low-Rise Apartments and Townhomes

RMF-S is typical Zoning Category	Current Regulation	Proposed Guidelines	Rationale
Lot Coverage	No current maximum	<ul style="list-style-type: none"> The maximum lot coverage shall be 42% in <u>Area 1</u>. <ul style="list-style-type: none"> Lot coverage may be increased beyond 42% where more than 50% of the required parking is provided underground. The maximum lot coverage shall be 50% in <u>Area 2</u>. <ul style="list-style-type: none"> Lot coverage may be increased beyond 50% where more than 50% of the required parking is provided underground. Dwellings may only reflect maximum lot coverage standards where applicable minimum setback and separation distance standards have been achieved. 	<ul style="list-style-type: none"> Lot coverage requirements will help to get better contextual fit. Additional considerations can then be given to greater coverage if underground parking is provided and the proposal adheres to other dimensional standard requirements.
Building Height	Current regulation: 35 feet tall in the RMF-S zoning category and 60 feet tall in the RMF-M zoning category	<p>New development should have building heights that are responsive to their context in order to mitigate negative impacts of larger buildings on adjacent properties and to facilitate integration into the neighbourhood. This is particularly important for neighbourhoods largely comprised of single-storey buildings. Building height maximums should be determined on the following basis:</p> <ul style="list-style-type: none"> The maximum building height for low-rise multi-family buildings shall be 45 feet on arterial streets; The maximum building height for low-rise buildings on the corner of local and a collector street should be 45 feet; and The maximum building height for low-rise buildings on local and collector streets should be 35 feet. 	<ul style="list-style-type: none"> Building heights should respond the to the street context within the neighbourhood. For example, higher order streets such as arterials are designed to accommodate larger buildings than local residential streets.

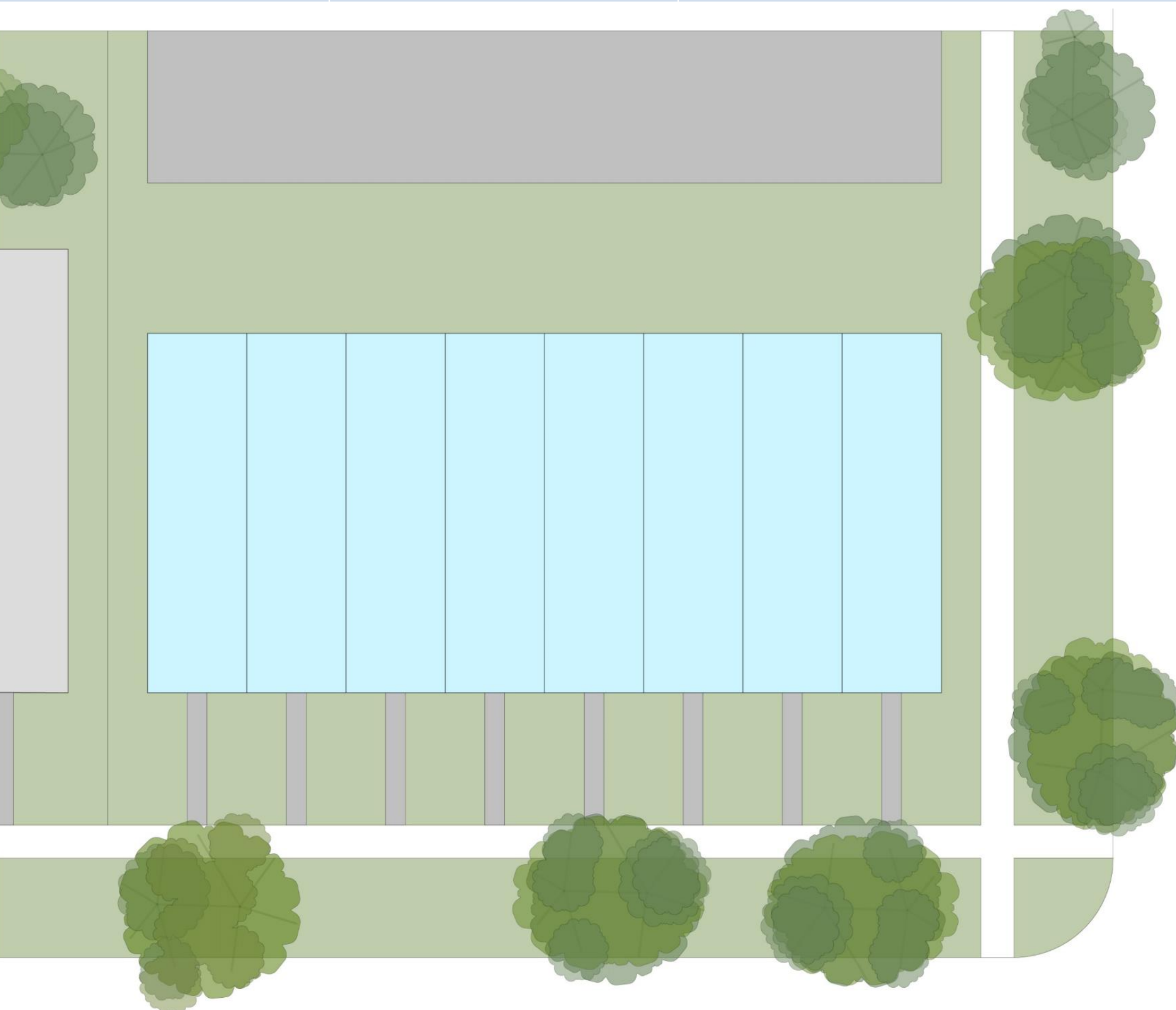


Figure 1 – demonstrates a townhouse dwelling in Area 2 with a lot coverage of 50% and surface parking

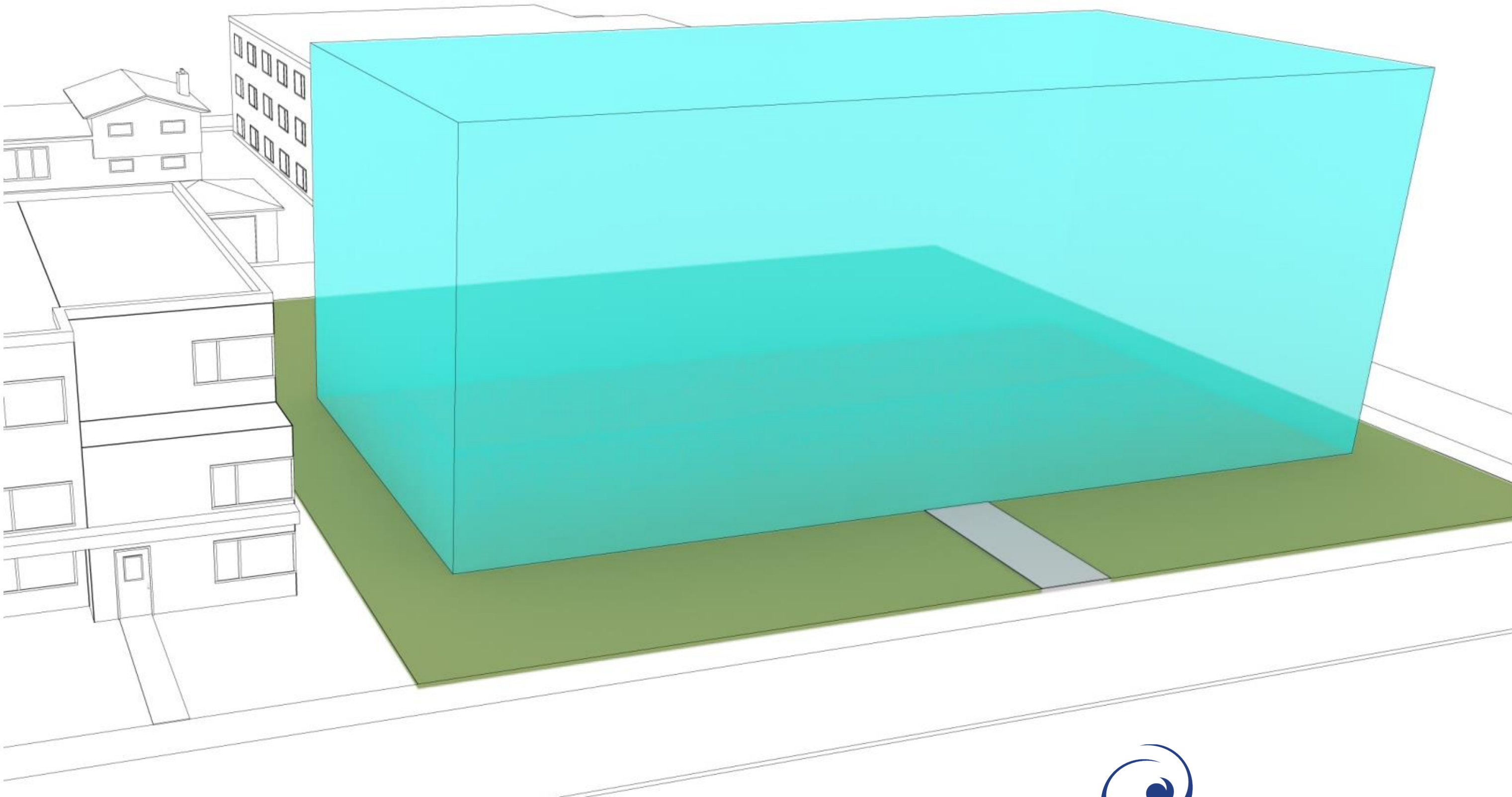


Figure 2 – demonstrates a low-rise apartment dwelling located on an arterial street with a building height of 45 feet.

Low-Rise Apartments and Townhomes

RMF-S is typical Zoning Category	Current Regulation	Proposed Guidelines	Rationale
Landscaping	<p>Building Foundation (1 shrub for every 10 feet of building)</p> <p>Street Edge (1 tree for each 30 feet and 3 shrubs for every 20 feet)</p> <p>Parking Lot Interior (1 tree for each 300 sq. ft. of parking area and 1 shrub for every 100 sq. ft.)</p>	<ul style="list-style-type: none">• New development should reflect and enhance the landscaping quality of the existing streetscape and adjacent properties.• New development should provide trees and shrubs on the following basis:<ul style="list-style-type: none">• 1 tree for every 20 linear feet; and• 3 shrubs for every 20 linear feet• New development should enhance the quality of the streetscape through the preservation and protection of healthy mature trees.• The requirement to provide trees and shrubs may be satisfied either through planting new or preserving existing trees on the following basis:<ul style="list-style-type: none">• Boulevard trees should not be removed or disturbed as a result of new development activity.• Tree protection measures, including fencing and root disturbance protection should be required as a condition of development permit.• A minimum of 20% of the lot area should be reserved for soft landscaping.• Provide adequate snow storage on site. Particular attention should be provided for parking areas.• Underground parking should not encroach into set back areas required for landscaping to ensure the long-term viability of mature trees and vegetation.	<ul style="list-style-type: none">• We heard that trees, shrubs and other natural features were significant character defining elements in a neighbourhood.• Increasing the quantity and quality of the landscaping for infill projects will better integrate buildings into the neighbourhood and improve the visual quality of the street.• Having incentives to keep healthy mature trees will help to protect our mature tree canopy and keep the character of our neighbourhoods.
Driveway Access, Parking and Garages		<ul style="list-style-type: none">• The amount of onsite parking should be sufficient to meet the needs of the development.• Parking relaxations may be considered.• Where a property abuts a lane, vehicular access will be required to be taken from the lane and no existing vehicular access from the roadway shall be permitted to continue. Any existing front approach must be removed and that the curb, sidewalk and boulevard be restored.• Where a property does not abut a lane, the driveway access may be from the adjacent street. Where a property is a corner lot, access should be from the flanking street. The location of the approach should not have a negative impact on mature trees within the public right-of-way, and its design should minimize conflict between vehicles and non-motorists and offer opportunity for shared driveway use.• Where property does not abut a lane, provide a minimum 10 feet wide landscape area and opaque fencing to buffer adjacent properties and amenity spaces from driveways and parking areas.	<ul style="list-style-type: none">• Parking and vehicle access are important design elements to consider to ensure compatible development.• Maintaining or re-establishing an uninterrupted sidewalk improves the pedestrian environment and safety by minimizing conflict with vehicles.• Over provision of on-site parking can have a detrimental impact on the look and feel of our mature neighbourhoods, many of which were design prior to the mass use of the automobile.

Low-Rise Apartments and Townhomes

RMF-S is typical Zoning Category	Current Regulation	Proposed Guidelines	Rationale
Building Design	No regulation	<p><i>Building materials</i></p> <ul style="list-style-type: none"> Any building façade facing a street or public space shall include at least two prominent materials. A prominent building façade material shall cover at minimum 25 percent of the front façade of the building. The front façade facing the primary street to which the building is oriented shall provide a minimum 25 percent as window area. On corner lots, the façade facing the flanking street shall provide a minimum of 20 percent as window area. <p><i>Articulation and Architectural Features</i></p> <ul style="list-style-type: none"> Articulation elements as well as architectural features should be used to create interest in the façade design. Ground floor building facades must include two architectural features for the portions of the building that face a street or public space. Architectural features may include, but are not limited to: Entrances with entry features such as porticos, awnings and other shading devices, porches, projections, horizontal architectural elements (e.g., masonry string courses, ledges, water tables), sculptural cladding, changes in the variety and size of windows. The second storey and above of any multi-storey building will require four architectural features for the portion of the building facing a street or public space. Architectural features may include, but are not limited to: projections, recesses, terraces or balconies, sculptural cladding and roof treatments. For corner lots, architectural features such as corner bays, wrap-around porches, towers, and/or additional glazing is encouraged to emphasize the corner condition. 	<ul style="list-style-type: none"> We heard concerns about the quality, look and design of infill homes and how they “fit” into the neighbourhood context. Adding design requirements for infill homes will improve the standard of infill housing while also ensuring new homes integrate better into the established context of the street.



Figure 1 – An example of a multi-unit dwelling that incorporates a variety of articulation and architectural features



Figure 2 – Another example of a multi-unit dwelling that incorporates a variety of articulation through the use of recesses, projections and balconies

Low-Rise Apartments and Townhomes

RMF-S is typical Zoning Category	Current Regulation	Proposed Guidelines	Rationale
Building Design continued...		<p><i>Length of building</i></p> <ul style="list-style-type: none"> Maximum building length along the street frontage will be considered on a case by case basis but generally should adhere to the following: <ul style="list-style-type: none"> For existing large sites, building length should not exceed 200 feet Where the character of the block is established with several smaller lots and lot consolidation is required, the length of the building should not exceed 120 feet in order to respect the context of the street, allow views through the site, and to mitigate building massing impacts on the block face. <p><i>Energy Performance</i></p> <ul style="list-style-type: none"> Consider building design that manages passive solar gain and improves energy performance where possible. This may be achieved through, but is not limited to efficient building envelope (cladding and insulation), glazing, window, door and balcony placement as well as roof design that can accommodate features such as green roofs, and solar panels. <p><i>Amenity Space</i></p> <ul style="list-style-type: none"> On sites where 20 or more units are proposed, communal amenity space shall be provided Low-rise apartments are encouraged to incorporate accessible communal outdoor amenity space on the roof, with the provision that it be designed in a manner that respects privacy and site lines between adjacent properties. 	<ul style="list-style-type: none"> Our mature neighbourhoods typically have lots and buildings that are more fined grained. This allows for greater diversity of housing, increased sightlines and sky views, and increased landscaping opportunities. Longer buildings on larger lots can have a negative impact on that established character. Providing guidelines on building length will help to ensure the streetscape character of mature neighbourhoods is protected. Amenity spaces are important features that have a positive impact on the quality of life of residents. This is particularly the case for developments where unit sizes are small in larger buildings.

Figure 1 – An example of a multi-unit residential dwelling where breaks are established between buildings to address building length



Location Criteria

Preferred Location Criteria and Secondary Considerations

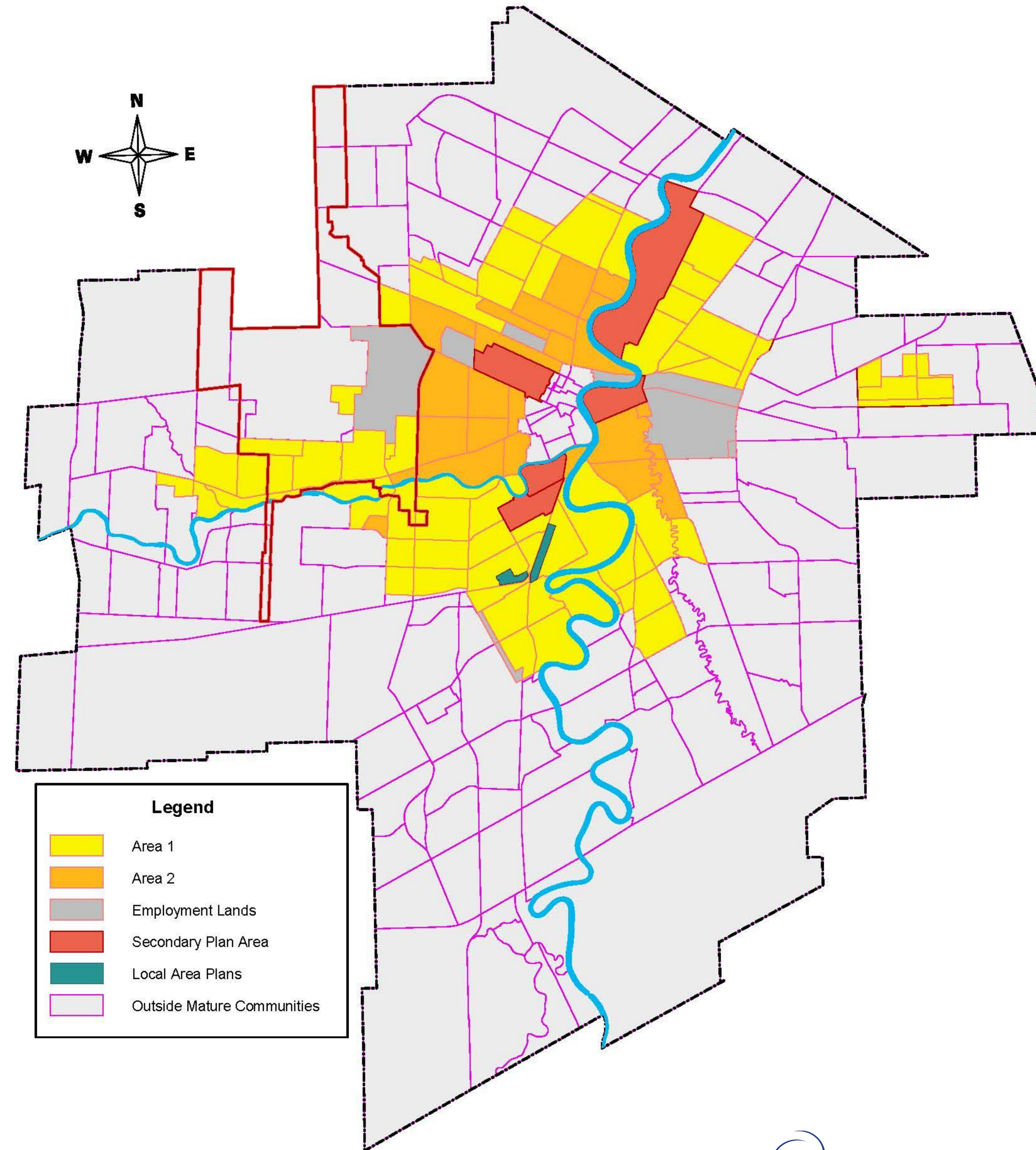
Location criteria refer to the parameters and conditions that will help guide what kind of building type is appropriate for different locations within our Mature Communities. There are two types of location criteria: Preferred Location Criteria and Secondary Considerations.

Preferred Location Criteria

Preferred Location Criteria outline specific conditions that should be present to support a particular type of development. The goal of the Preferred Location Criteria is to outline the scenario where certain types of buildings should be supported. These criteria generally address requirements for minimum lot sizes, location on the block, and proximity to transit and other uses.

Secondary Considerations

Secondary Considerations are less specific and may be broadly considered to determine whether or not a particular project is appropriate for that location. City administration will use the Secondary Considerations as a guide to review land development applications when a proposed project does not meet the Preferred Location Criteria.



Preferred Location Criteria

Housing Typology	Current Regulation	Proposed Guidelines	Rationale
Subdivision of one lot into two	Reviewed on a case-by-case basis often requiring variances and a public hearing for approval.	<ul style="list-style-type: none">In area 1 and 2 a lot with a rear lane may be subdivided into two (2) equal sized lots and shall refer to the minimum site width requirements of the Zoning By-law. For lots without access to a rear lane, the minimum site width shall not be less than 40 feet.A lot may not be subdivided into more than two lots, notwithstanding the minimum site width requirements of the Zoning By-law. Subdivision into more than two lots may only be approved where the proposed subdivision is in character with lots on the same block.	<ul style="list-style-type: none">Single family dwellings on narrow lots can provide moderate density in a neighbourhood and provide modern housing options for families in a manner that is compatible with other single family dwellings.Single family dwellings on narrow lots should be able to achieve a design that fits within the context of the nearby properties when designed in conjunction with the proposed design guidelines.
Up-down duplex dwellings	Permitted in the R2 zoning district	<ul style="list-style-type: none">On all properties in areas 1 and 2 an up-down duplex use should be permitted on sites with access to a rear lane, a minimum lot width of 32 feet and a minimum lot area of 2,880 square feet.	<ul style="list-style-type: none">Up-down duplex dwellings provide moderate changes in density and greater housing options for people in a manner that is compatible with single family housing.When considered in a manner that aligns with the design guidelines, up-down two-unit dwellings should be able to achieve a design that fits within the context of nearby properties.
Side-by-side dwellings	Permitted in the R2 zoning district	<ul style="list-style-type: none">On all properties in areas 1 and 2 a side-by-side two-unit dwelling should be permitted on sites that have a rear lane, a minimum site width of 50 feet and a minimum lot area of 4,500 square feet.Secondary suites should be permitted.	<ul style="list-style-type: none">Side-by-side two unit dwellings on a 50 foot wide lot are similar to a 50 foot wide lot that is subdivided for single family dwellings.Side-by-side dwellings can provide the same built form as a large single family dwelling.

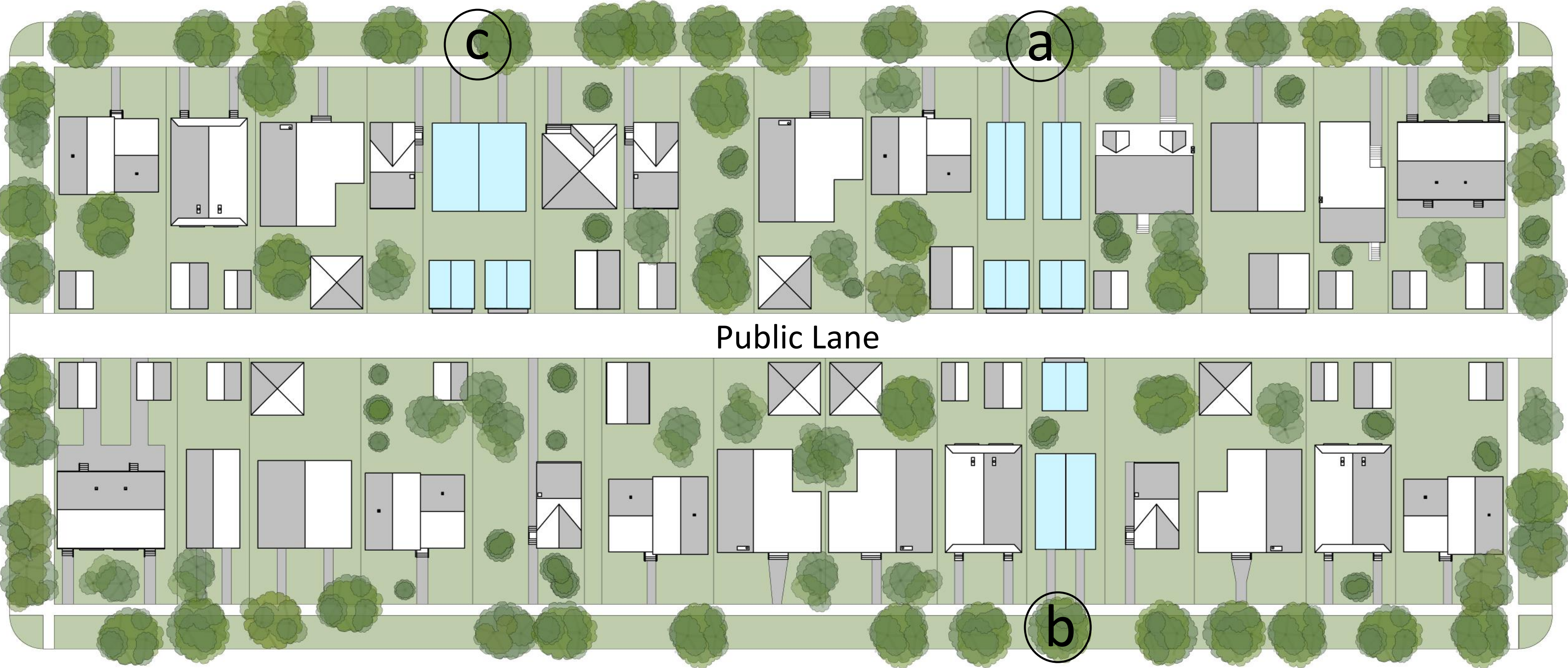


Figure 1 – demonstrates location criteria for:
(a) a subdivision of one lot into two lots;
(b) up-down duplex; and,
(c) Side-by-side dwellings
Applicable to both Area 1 and Area 2

Preferred Location Criteria

Housing Typology	Current Regulation	Proposed Guidelines	Rationale
Three-unit dwellings (e.g. triplexes)	Permitted in R2 zoning district (with a conditional use application) and RMF zoning districts	The preferred location criteria for a three-unit building in Area 1 is where all of the following criteria are met: <ul style="list-style-type: none">On sites where the rear yard abuts a lane;On sites with a minimum site width of at least 35 feet (10.66 metres) and a minimum lot area of 3,500 square feet (325.16 square metres);On sites located within 2,625 feet (800 metres) of the primary transit network or rapid transit station; andOn corner sites (external and internal corners).	<ul style="list-style-type: none">We heard that three-unit buildings could fit into a low density neighbourhood when located on corner lots and within close proximity to the primary transit network.The preferred location criteria for three-unit buildings within Area 1 are being recommended as a means to gently introduce this form of housing into neighbourhoods that are established with predominantly single family dwellings.When considered in a manner that aligns with the design guidelines, three-unit buildings should be able to achieve a design that fits within the context of nearby properties.
		The preferred location criteria for a three-unit building in Area 2 is where all of the following criteria are met: <ul style="list-style-type: none">On sites where the rear yard abuts a lane; andOn sites with a minimum site width of 35 feet (10.66 metres)	<ul style="list-style-type: none">We heard that three-unit dwellings might be introduced more readily within these types of neighbourhoods (e.g., mid block properties)The preferred location criteria for three-unit within Area 2 are being recommended as a means to introduce this form of housing into neighbourhoods that are already established with a variety of housing types and zoning is conducive.When considered in a manner that aligns with the design guidelines, three-unit buildings should be able to achieve a design that fits within the context of nearby properties.

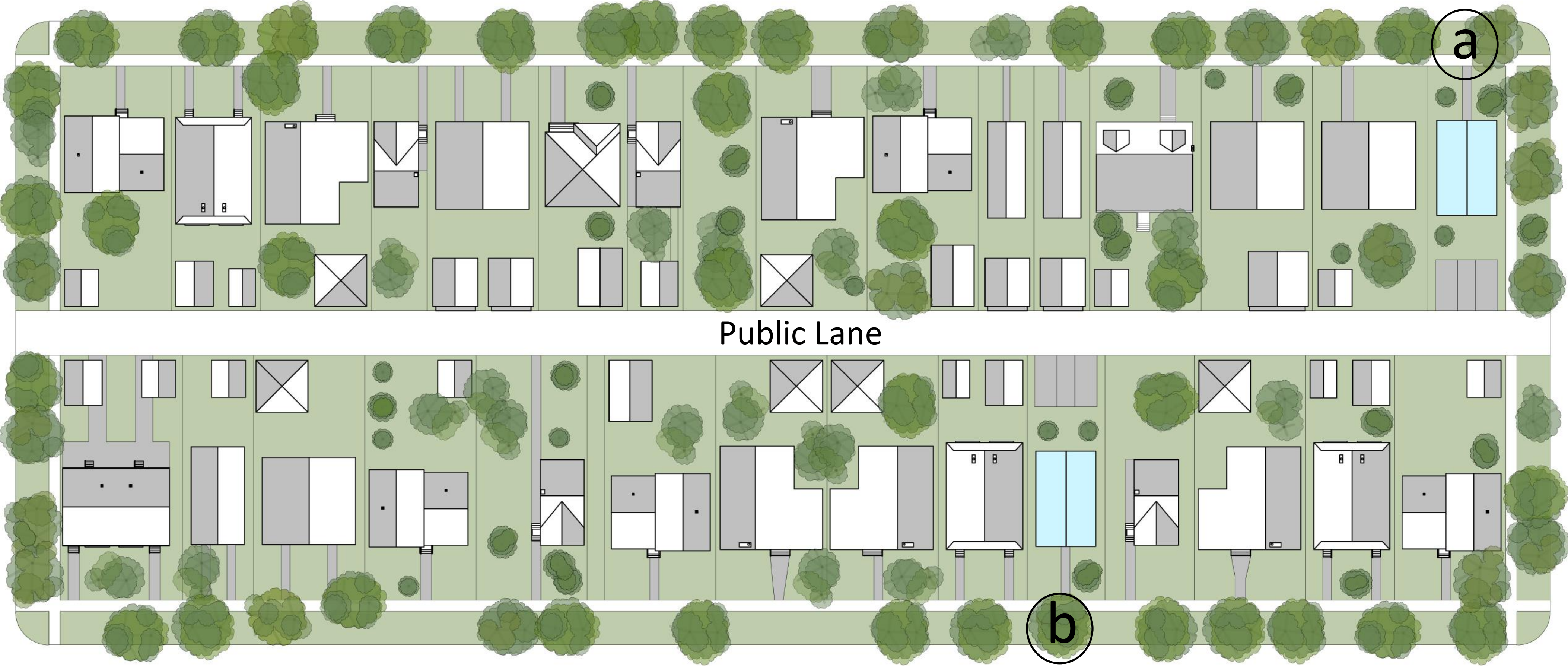


Figure 1 – demonstrates location criteria for a three unit dwelling in Area 1 (a) and Area 2 (b)

Preferred Location Criteria

Housing Typology	Current Regulation	Proposed Guidelines	Rationale
Four-unit dwellings (e.g. fourplexes)	Permitted in R2 zoning district (with a conditional use application) and RMF zoning districts	<p>The preferred location criteria for a four-unit building in Area 1 is where all of the following criteria are met:</p> <ul style="list-style-type: none">• On sites where the rear yard abuts a lane;• On sites with a minimum site width of at least 50 feet (15.24 metres) and a minimum lot area of 5,000 square feet (464.52 square metres);• On sites located within 2,625 feet (800 metres) of the primary transit network or rapid transit station; and• On corner sites (external and internal corners);	<ul style="list-style-type: none">• We heard that four-unit buildings could fit into a low density neighbourhood when located on corner lots and within close proximity to the primary transit network.• The preferred location criteria for four unit buildings within Area 1 are being recommended as a means to gently introduce this form of housing into neighbourhoods that are established with predominantly single family dwellings.• When considered in a manner that aligns with the design guidelines, four-unit buildings should be able to achieve a design that fits within the context of nearby properties.
		<p>The preferred location criteria for a four-unit building in Area 2 is where all of the following criteria are met:</p> <ul style="list-style-type: none">• On sites where the rear yard abuts a lane; and• On sites with a minimum site width of 50 feet (15.24 metres).	<ul style="list-style-type: none">• We heard that four-unit buildings might be introduced more readily within these types of neighbourhoods (e.g., mid block properties).• The preferred location criteria for four-unit buildings within Area 2 are being recommended as a means to introduce this form of housing into neighbourhoods that are already established with a variety of housing types and zoning is conducive.• When considered in a manner that aligns with the design guidelines, four-unit buildings should be able to achieve a design that fits within the context of nearby properties.

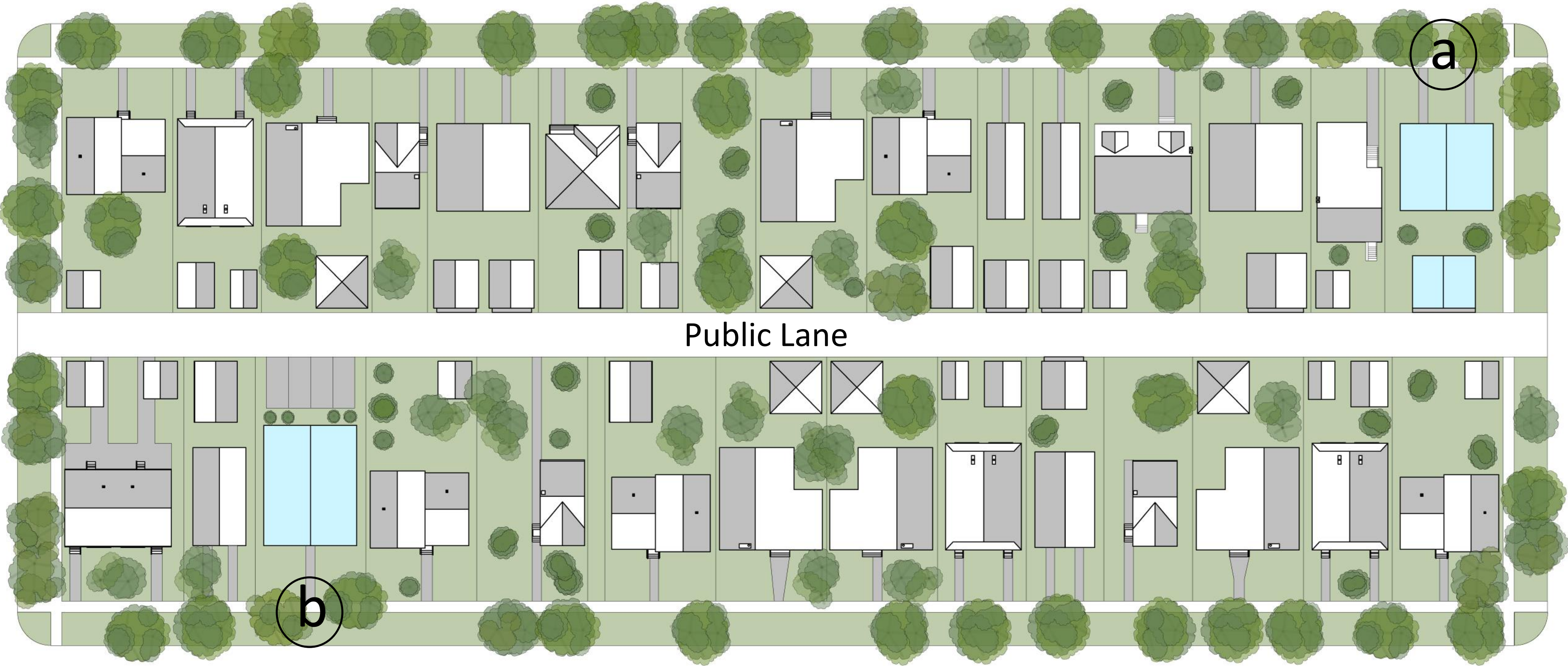


Figure 1 – demonstrates location criteria for a four unit dwelling in Area 1 (a) and Area 2 (b)

Preferred Location Criteria

Housing Typology	Current Regulation	Proposed Guidelines	Rationale
Townhouse	Permitted in R2 zoning district (with a conditional use application) and RMF zoning districts	<p>The preferred location criteria for townhouse buildings in Area 1 should be on the following basis:</p> <p><u>Scenario 1</u></p> <ul style="list-style-type: none">On sites where the rear yard abuts a lane;On sites located within 1,312 feet (400 metres) of the primary transit network or rapid transit station; andWhere the property abuts an arterial street; <p>or</p> <p><u>Scenario 2</u></p> <ul style="list-style-type: none">On sites where the rear yard abuts a lane;On sites located within 1,312 feet (400 metres) of the primary transit network or rapid transit station; andWhere two or more of the following conditions are met:<ul style="list-style-type: none">i. On corner sites;ii. On sites where the rear yard is separated by lane from a commercial, multi-family or non-residential use; oriii. On sites where at least one side yard abuts a commercial, multi-family or non-residential use.	<ul style="list-style-type: none">We heard that townhouse dwellings could best fit into a low density neighbourhood when located on busier streets, where there were similar buildings nearby, when they are located on corner lots and when they were located within close proximity to the primary transit network.The preferred location criteria for townhomes within Area 1 are being recommended as a means to gently introduce this form of housing into neighbourhoods that are established with predominantly single family dwellings.When considered in a manner that aligns with the design guidelines, townhouse dwellings should be able to achieve a design that fits within the context of nearby properties.

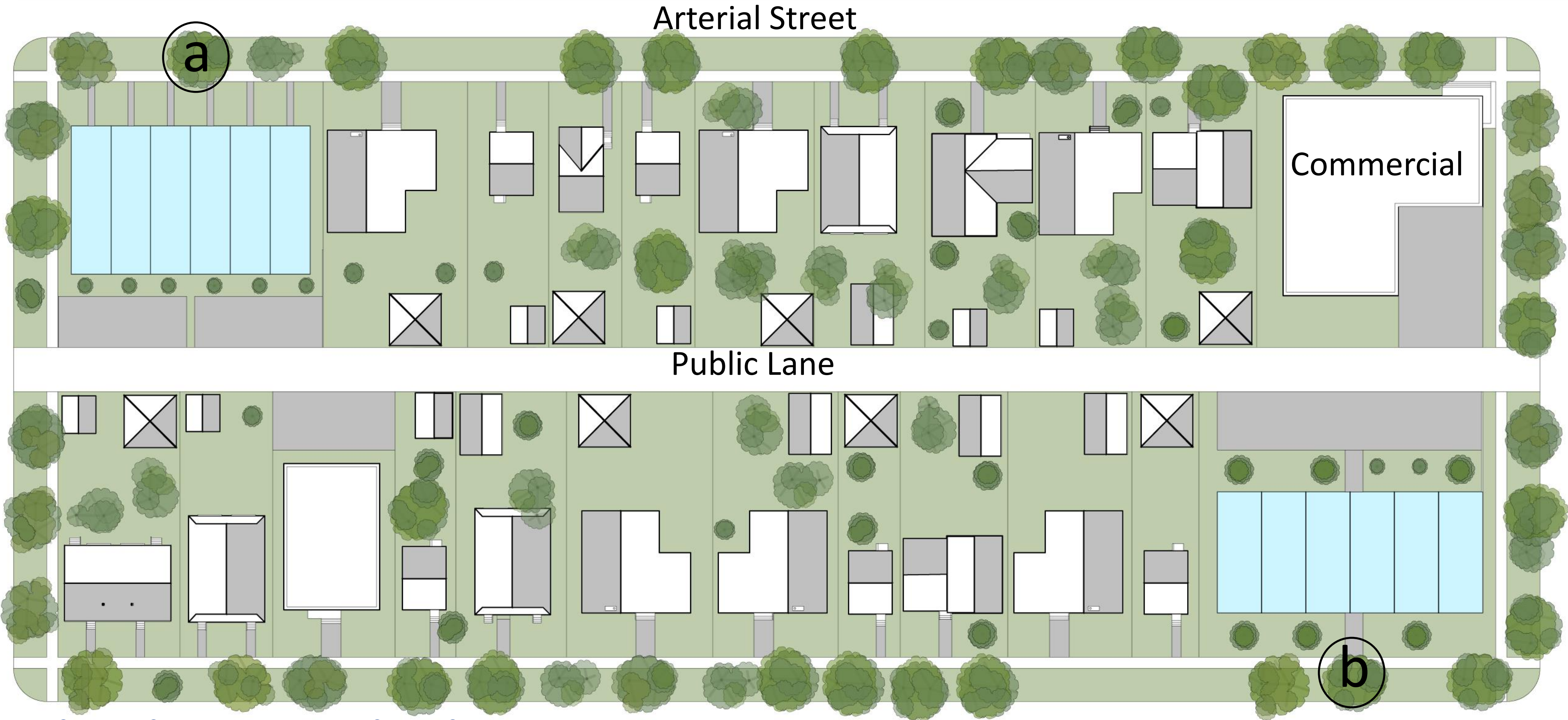


Figure 1 – demonstrates location criteria for townhouse dwellings in Area 1 for Scenario 1 (a) and Scenario 2 (b)

Preferred Location Criteria

Housing Typology	Current Regulation	Proposed Guidelines	Rationale
Townhouse	Permitted in R2 zoning district (with a conditional use application) and RMF zoning districts	<p>The preferred location criteria for townhouse buildings in Area 2 should be on the following basis:</p> <p><u>Scenario 1</u></p> <ul style="list-style-type: none">On sites where the rear yard abuts a lane;On sites located within 1,312 feet (400 metres) of the primary transit network or rapid transit station; andWhere the property abuts an arterial street; <p>or</p> <p><u>Scenario 2</u></p> <ul style="list-style-type: none">On sites where the rear yard abuts a lane;On sites located within 1,312 feet (400 metres) of the primary transit network or rapid transit station; andWhere one or more of the following conditions are met:<ul style="list-style-type: none">i. On corner sites;ii. On sites where the rear yard is separated by lane from a commercial, multi-family or non-residential use; oriii. On sites where at least one side yard abuts a commercial, multi-family or non-residential use.	<ul style="list-style-type: none">We heard that townhouse dwellings could be more readily accommodated within these types of neighbourhoods given the existing mix of uses, proximity to commercial services and the primary transit network.The preferred location criteria for townhouse dwellings within Area 2 are being recommended as a means to continue to incorporate this form of housing into neighbourhoods that are already established with a variety of housing types and where the zoning is conducive to allowing for a greater variety of housing types.When considered in a manner that aligns with the design guidelines, townhouses should be able to achieve a design that fits within the context of nearby properties.

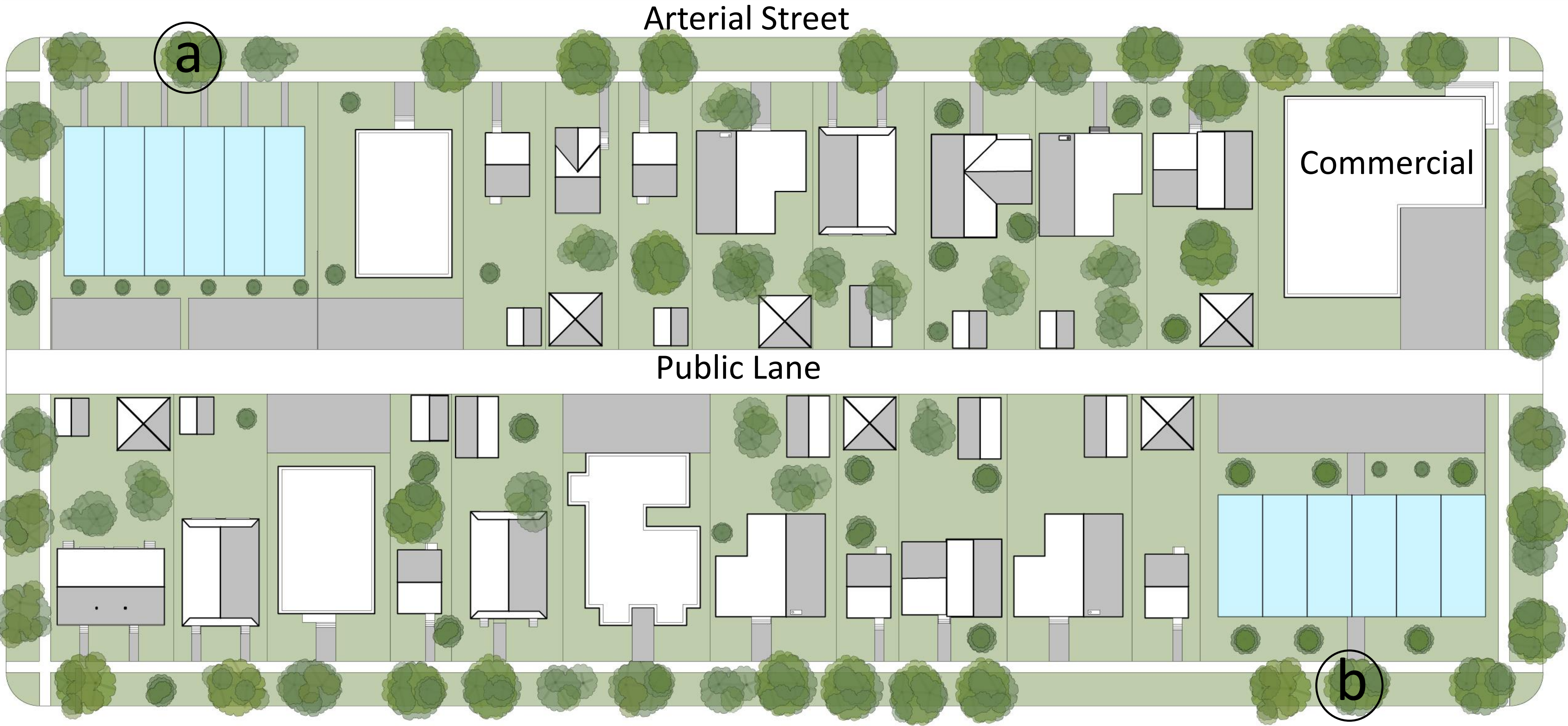


Figure 1 – demonstrates location criteria for townhouse dwellings in Area 2 for Scenario 1 (a) and Scenario 2 (b)

Preferred Location Criteria

Housing Typology	Current Regulation	Proposed Guidelines	Rationale
Low-Rise Multi Unit Dwelling	Permitted in R2 zoning district (with a conditional use application) and RMF zoning districts	<p>The preferred location criteria for low-rise multi-unit buildings in Area 1 should be on the following basis:</p> <p><u>Scenario 1</u></p> <ul style="list-style-type: none">On sites where the rear yard abuts a lane;On sites located within 1,312 feet (400 metres) of the primary transit network or rapid transit station; andWhere the property abuts an arterial street; <p>or</p> <p><u>Scenario 2</u></p> <ul style="list-style-type: none">On sites where the rear yard abuts a lane;On sites located within 1,312 feet (400 metres) of the primary transit network or rapid transit station; andWhere two or more of the following conditions are met:<ul style="list-style-type: none">i. On corner sites;ii. On sites where the rear yard is separated by lane from a commercial, multi-family or non-residential use; oriii. On sites where at least one side yard abuts a commercial, multi-family or non-residential use.	<ul style="list-style-type: none">We heard that townhouse dwellings could best fit into a low density neighbourhood when located on busier streets, where there were similar buildings nearby, when they are located on corner lots and when they were located within close proximity to the primary transit network.The preferred location criteria for low rise multi unit dwellings within Area 1 are being recommended as a means to gently introduce this form of housing into neighbourhoods that are established with predominantly single family dwellings.When considered in a manner that aligns with the design guidelines, low rise multi unit dwellings should be able to achieve a design that fits within the context of nearby properties.

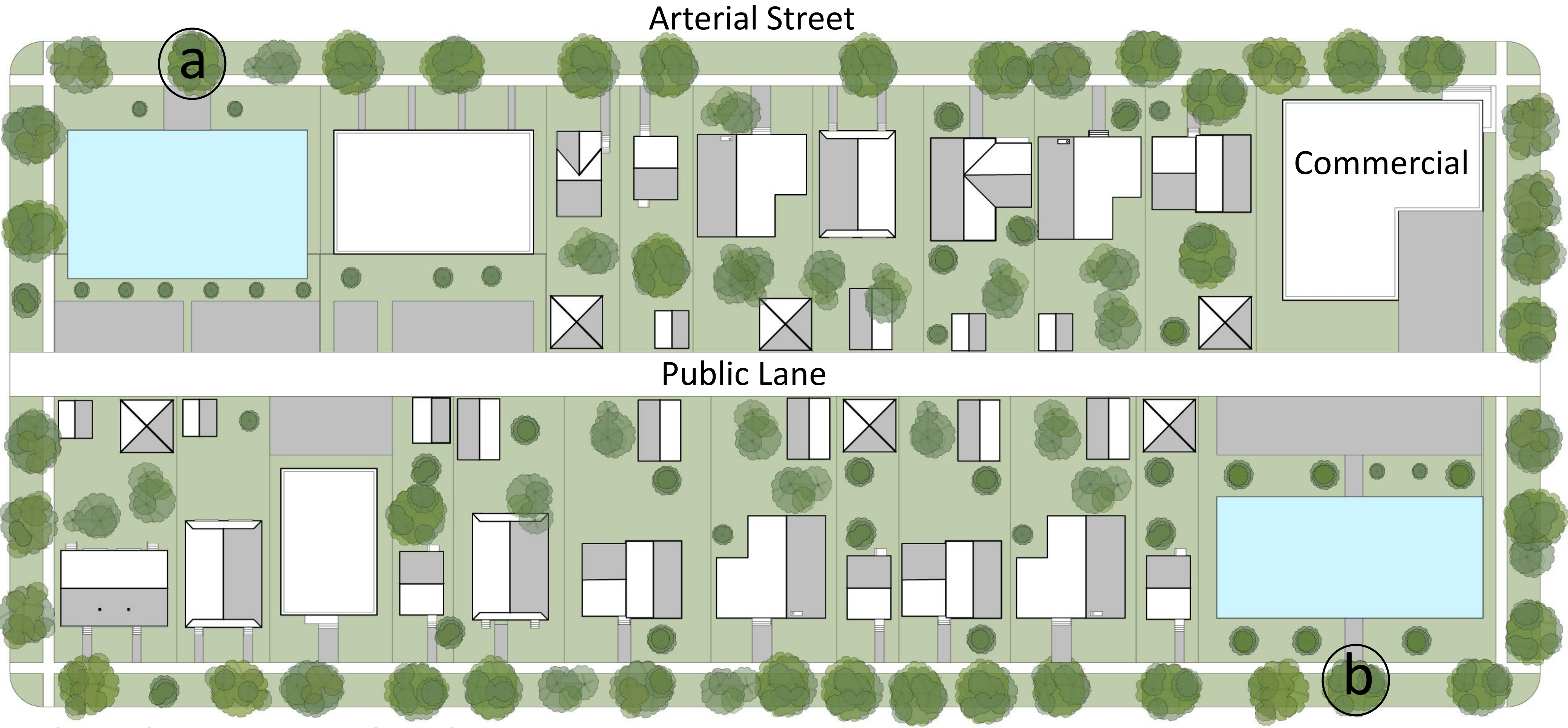


Figure 1 – demonstrates location criteria for Low-Rise Multi Unit dwellings in Area 1 for Scenario 1 (a) and Scenario 2 (b)

Preferred Location Criteria

Housing Typology	Current Regulation	Proposed Guidelines	Rationale
Low-Rise Multi Unit Dwelling	Permitted in R2 zoning district (with a conditional use application) and RMF zoning districts	<p>The preferred location criteria for low-rise multi-unit buildings in Area 2 should be on the following basis:</p> <p><u>Scenario 1</u></p> <ul style="list-style-type: none">On sites where the rear yard abuts a lane;On sites located within 1,312 feet (400 metres) of the primary transit network or rapid transit station; andWhere the property abuts an arterial street; <p>or</p> <p><u>Scenario 2</u></p> <ul style="list-style-type: none">On sites where the rear yard abuts a lane;On sites located within 1,312 feet (400 metres) of the primary transit network or rapid transit station; andWhere one or more of the following conditions are met:<ul style="list-style-type: none">i. On corner sites;ii. On sites where the rear yard is separated by lane from a commercial, multi-family or non-residential use; oriii. On sites where at least one side yard abuts a commercial, multi-family or non-residential use.	<ul style="list-style-type: none">We heard that low-rise multi-unit dwellings could be more readily accommodated within these types of neighbourhoods given the existing mix of uses, proximity to commercial services and the primary transit network.The preferred location criteria for low-rise multi-unit dwellings within Area 2 are being recommended as a means to continue to incorporate this form of housing into neighbourhoods that are already established with a variety of housing types and where the zoning is conducive to allowing for a greater variety of housing types.When considered in a manner that aligns with the design guidelines, low-rise multi-unit dwellings should be able to achieve a design that fits within the context of nearby properties.

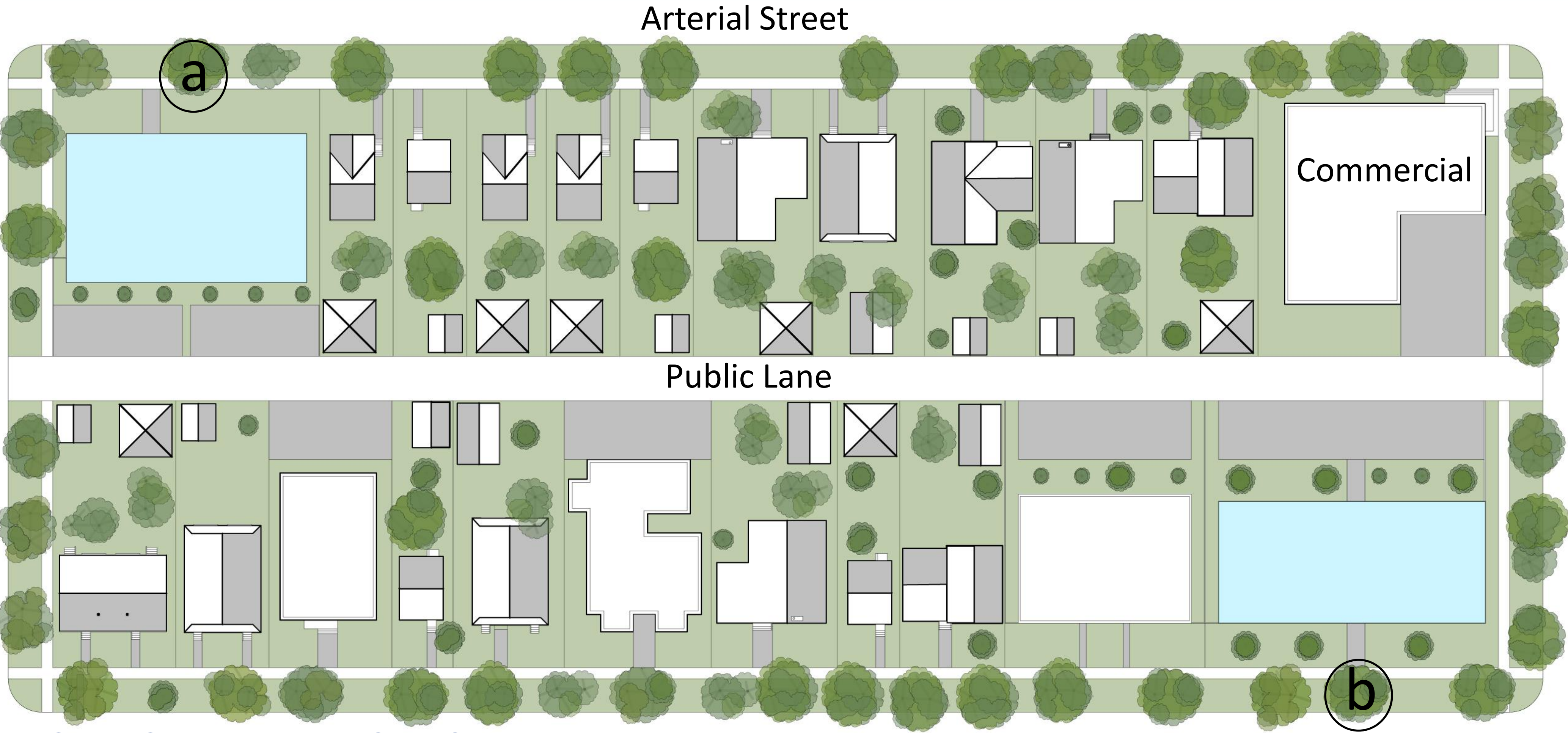


Figure 1 – demonstrates location criteria for Low-Rise Multi Unit dwellings in Area 2 for Scenario 1 (a) and Scenario 2 (b)

Location Criteria – Secondary Considerations

Secondary Considerations are less specific and may be broadly considered to determine whether or not a particular project is appropriate for that location. City administration will use the Secondary Considerations as a guide to review land development applications when a proposed project does not meet the Preferred Location Criteria.

Every land development application will be reviewed on a case-by-case basis. Each one will be assessed on the scale and type of development proposed in relation to the context of the neighbourhood around it and how closely it aligns with the intent of these Guidelines.

Housing Typology	Current Regulation	Secondary Considerations	Rationale
All Housing Types	N/A	Existing zoning of the property and adjacent properties	Considering the existing zoning of the property and that of adjacent properties provides an understanding of the development context of the street and its immediate development potential.
All Housing Types	N/A	Characteristics of the immediately surrounding built form, including building mass, height, lot coverage, setbacks, and layout	Considering the scale and massing of development nearby provides an understanding of the context of the street and its immediate development potential. Ensuring that new development is contextually suitable requires an appropriate transition in building heights, massing, and intensification from areas with greater development intensities to areas with lower development intensities.
All Housing Types	N/A	Surrounding uses and their characteristics, including residential density and the intensity of commercial and other non-residential uses	Considering the proximity to existing daily amenities (e.g., schools, commercial services, day cares, etc.) provides an understanding of development potential that helps to meet <i>OurWinnipeg</i> and the <i>Complete Communities Direction Strategy 2.0</i> ’s objectives of building more complete communities and reducing vehicle miles traveled.



Figure 1 – Demonstrates a scenario where a established built form or underlying zoning may have permitted a mix of housing and commercial uses that might be conducive to higher density development on adjacent properties.



Figure 2 – Demonstrates a block where lower density single family and multi unit residential are established.

Location Criteria – Secondary Considerations

Housing Typology	Current Regulation	Proposed Guidelines	Rationale
All Housing Types	N/A	The supporting street network	Considering the supporting street network provides an understanding of the immediate development potential. Placing higher density residential developments on roads which can accommodate higher traffic volumes (i.e., arterial and collector streets) can help mitigate the impact of increased traffic on local residential streets.
All Housing Types	N/A	The supporting transit, pedestrian, and active transportation network	Encouraging a variety of housing types with higher densities to locate near primary transit network stops and active transportation infrastructure has many benefits. These include supporting our municipal investments in transit and reducing our reliance on vehicles, which will in turn reduce our greenhouse gas emissions.
All Housing Types	N/A	Municipal servicing capacity	Encouraging sustainable development includes understanding whether the existing municipal services (e.g. water, sewer and land drainage) are able to meet the needs of the proposed development.
All Housing Types	N/A	Characteristics of the lot	Considering the characteristics of the development site provides an understanding of development potential. Certain lots have characteristics that make it easier to accommodate larger buildings or higher densities than is typical within a neighbourhood.
All Housing Types	N/A	Proximity to other policy areas	Considering the close proximity of other policy areas within the <i>Complete Communities Direction Strategy 2.0</i> , such as Transformative Areas, including Mixed Use Centres, Mixed Use Corridors, Major Redevelopment Sites, and the Downtown area provides an understanding of development potential. Encouraging low-rise multi-unit residential developments in close proximity to these higher density policy areas helps promote suitable transitions in built form from higher density areas to lower density areas.

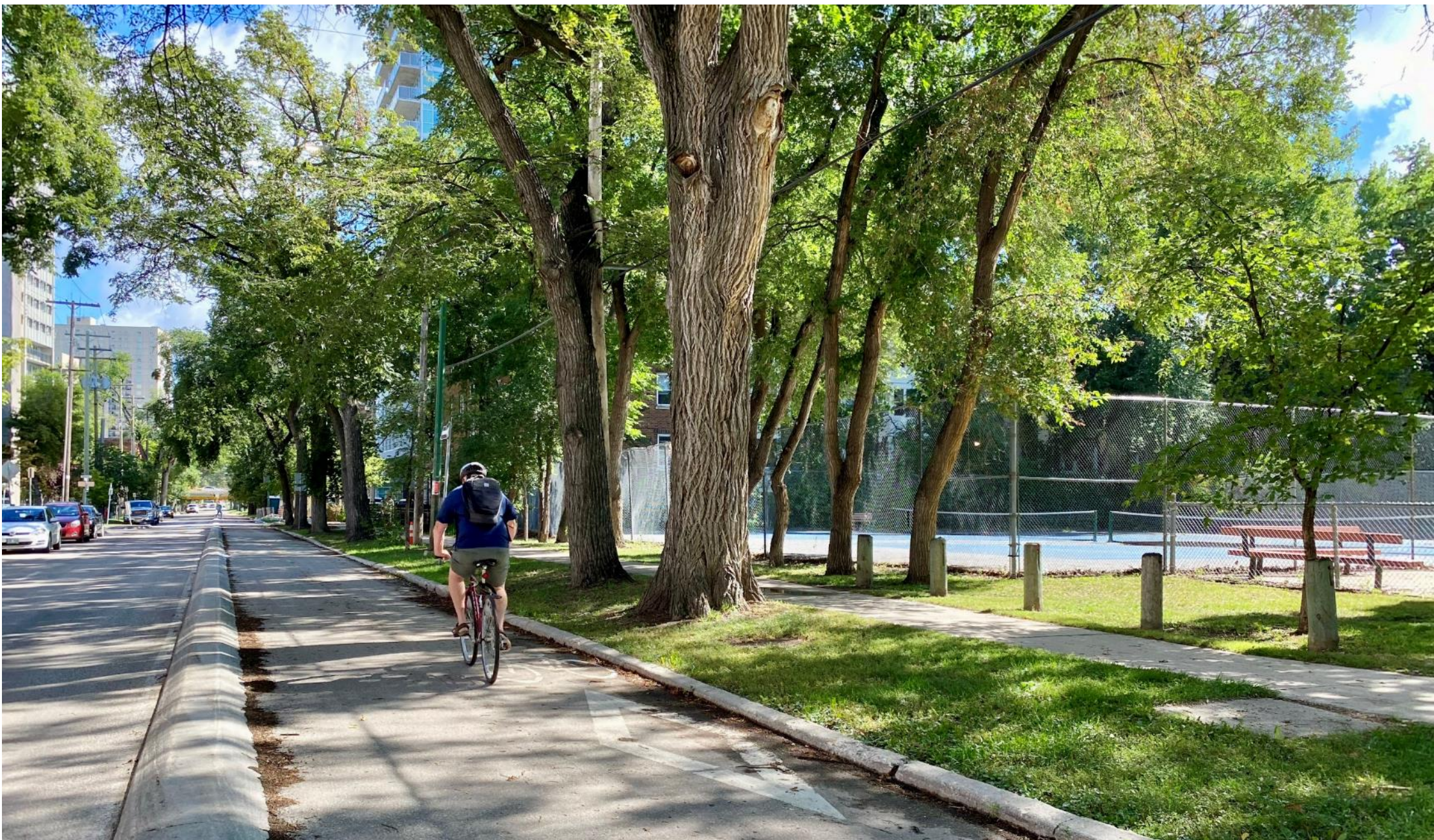


Figure 1 – Example of a bike way that supports safe cycling opportunities which can encourage people to use alternate forms of transportation to automobile use.

Thank you

**Please complete a survey if you
have feedback**