

**LEGEND**

PROPOSED CENTRELINE TRACK	
PROPOSED POINT OF SWITCH	
PROPOSED TURNOUT UNIT	
EXISTING CENTRELINE TRACK	
EXISTING CULVERT	
EXISTING OVERHEAD POWERLINE (APPROXIMATE LOCATION SHOWN)	
EXISTING MANHOLE	
EXISTING ELECTRICAL BOX	
EXISTING GROUND	
EXISTING AQUEDUCT	
PROPOSED FOULING POINT	
EXISTING MILE POST	
GWWD ROW	
PROPOSED PROPERTY ACQUISITION	
PROPOSED CENTRELINE DITCH	
GEOTEXTILE SEPARATION FABRIC	
GRADING LIMIT	
500mm STRIPPING	
TEMPORARY EASEMENT AREA FOR ACCESS ROAD CONSTRUCTION	
EXISTING CONTOURS AND LABELS DISPLAYED AT 0.5m INTERVAL	

**LIST OF PROJECT DRAWINGS**

CT-000	COVER SHEET
CT-001	INDEX PAGE
CT-101	GENERAL LAYOUT
CT-201	PROPOSED SIDING TRACK PLAN & PROFILE
CT-301	TYPICAL SECTIONS & DETAILS
CT-401	CROSS SECTIONS - STA. 0+000 TO STA. 0+140
CT-402	CROSS SECTIONS - STA. 0+150 TO STA. 0+261

**ABBREVIATIONS**

BVCE	BEGIN VERTICAL CURVE ELEVATION
BVCS	BEGIN VERTICAL CURVE STATION
CL	CENTERLINE
C.O.W.	CITY OF WINNIPEG
CT	CURVE TO TANGENT
Dc	DEGREE OF CURVE
DS	DOUBLE SHOULDERED
ELEV	ELEVATION
EVCE	END VERTICAL CURVE ELEVATION
EVCS	END VERTICAL CURVE STATION
EX.	EXISTING
GRD.	GROUND
LH T.O.	LEFT HAND TURNOUT
LVC	LENGTH OF VERTICAL CURVE
L.S.T	LAST LONG TIE
Mi.	MILEAGE
ML	MAINLINE
N.T.S.	NOT TO SCALE
PI	POINT OF INTERSECTION
PRPSD.	PROPOSED
P.S.	POINT OF SWITCH
PVI	POINT OF VERTICAL INTERSECTION
RH T.O.	RIGHT HAND TURNOUT
RBM	RAIL BOUND MANGANESE
STA.	STATION
SUB.	SUBDIVISION
TC	TANGENT TO CURVE
T.O.	TURNOUT
TOR	TOP OF RAIL
TYP.	TYPICAL

**GENERAL NOTES**

1. ALL DIMENSIONS ARE IN METERS UNLESS NOTED OTHERWISE.
2. SURVEY CONTROL IS BASED ON GEODETIC DATUM NAD83 (CSRS)- UTM ZONE 14U.
3. CONTRACTOR IS RESPONSIBLE FOR LOCATING AND PROTECTING ALL BURIED UTILITIES WHETHER IDENTIFIED OR NOT.
4. ALL EXISTING FENCES AFFECTED BY THE CONSTRUCTION ARE TO BE REBUILT BY THE CONTRACTOR AS DIRECTED BY THE ENGINEER IN THE FIELD.
5. SOME LOCAL DITCH IMPROVEMENT MAY BE REQUIRED TO ENSURE DRAINAGE IS MAINTAINED - AS DIRECTED BY THE ENGINEER IN FIELD.
6. ALL CONSTRUCTION EQUIPMENT OR MATERIAL SHALL BE KEPT TO THE NORTH SIDE OF THE MAIN LINE. NO WORK SHALL BE DONE SOUTH OF THE TRACKS IN PROXIMITY TO THE AQUEDUCT.
7. UNLESS NOTED OTHERWISE IN THE CONTRACT DOCUMENTS, ALL MATERIAL AND CONSTRUCTION SHALL CONFORM TO CN ENGINEERING SPECIFICATIONS FOR INDUSTRIAL TRACKS (NOVEMBER 2019) AND STANDARDS SET FORTH IN AREMA MANUAL FOR RAILWAY ENGINEERING (2022).
8. UNLESS DIRECTED OTHERWISE, CONTRACTOR IS REQUIRED TO NOTIFY GWWD DISPATCH UPON ARRIVAL AND DEPARTURE FROM SITE BY PHONING 204-986-4175 OR 204-330-3397.
9. ACCESS TO THE SITE IS PROVIDED THROUGH TEMPORARY CONSTRUCTION EASEMENTS ON PRIVATELY OWNED LAND, AND THROUGH LAND PURCHASE AGREEMENTS THAT HAVE NOT YET BEEN FINALIZED. THE CONTRACTOR SHALL RESTORE ALL DAMAGED PROPERTY AND AGRICULTURAL LAND TO ITS ORIGINAL CONDITION WITHIN THE TEMPORARY EASEMENT SHOWN ON DWG. CT-101.

**TRACK DESIGN NOTES**

1. PROPOSED SIDING TRACK DESIGN SPEED = 10 MPH.
2. MAXIMUM DEGREE OF CURVE = 7° 30' 00"
3. RAIL CAR LENGTH = 60' (18.29m)
4. SIDING TRACK IS NON-SUPERELEVATED.
5. 14 ft. TRACK CENTRE AT THE FOULING POINT.

B.M. ELEV.	 500 - 311 Portage Avenue, Winnipeg MB Canada www.stantec.com			THE CITY OF WINNIPEG WATER AND WASTE DEPARTMENT ENGINEERING DIVISION		
				WINNIPEG-MANITOBA		SHEET 2 OF 7
				GREATER WINNIPEG WATER DISTRICT RAILWAY PROPOSED SIDING RELOCATION INDEX PAGE		CITY DRAWING NUMBER 1-0751R-D0005-001-R0
				CONSULTANT DRAWING NUMBER CT-001		
DESIGNED BY MY	CHECKED BY MY	SCALE: HORIZONTAL N.T.S. VERTICAL	APPROVED BY ERT	RELEASED FOR CONSTRUCTION		
ISSUED FOR TENDER 0	DATE 23/05/05	M.Y.	DATE 2023 05 05	DATE		
NO.	REVISIONS	DATE	BY			

BID OPPORTUNITY: 57-2023  
CONTRACT NUMBER: X

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