



**City of Winnipeg
Planning, Property and Development Department**

**Invitation for Expressions of Interest for the
Leasing and Development of City
owned Riverbank Property on Waterfront Drive –
Alexander Dock and Harbourmaster Building
Sites**

Winnipeg, Manitoba

(EOI # 679 – 2006)

1.0 INTRODUCTION

The City of Winnipeg (the City) invites Expressions of Interest (EOI) from Proponents, with respect to the leasing and development of City-owned riverbank properties in accordance with the attached Alexander Docks Vision and Development Parameters in Schedule A.

Proponents may submit Expressions of Interest for Site “A” or Site “B” or both sites, as shown on Misc. Plan 12596/1 and attached as Schedule B, and herein after referred to as the Property. The Property consists of both sites and building situated on Site B.

The Invitation for Expressions of Interest does not include the Alexander Dock Structure as shown on Schedule B.

This information package provides a brief overview of the Property and the associated terms of reference for this EOI.

2.0 LOCATION

The Property is located north of Stephen Juba Park and east of Waterfront Drive within the East Exchange District.

For the purposes of this EOI, the Property is herewith used to describe the Harbourmaster building and adjacent riverbank lands, all shown outlined as Proposed Development Site on Misc. Plan No. 12596/1 attached as Schedule B.

The subject lands are approximately ±3.0 acres of City owned property.

As identified on the attached Misc. Plan No. 12596/1 included within Schedule B, the property, buildings and lands are located at 339 Waterfront Drive. The Harbourmaster building is approximately +/- 4327 square feet in size and has three levels. The main floor level is approximately +/- 2,556 sq. ft.

Paddlewheel Riverboats Ltd. have used the lands situated immediately west of the Alexander Dock structure for patron and operations support vehicle parking. The City is now prepared to consider alternate uses and development of the property but intends to continue leasing the Alexander Dock structure to Paddlewheel Riverboats Ltd. in the future.

3.0 BACKGROUND

i) Alexander Docks

The Federal Government proceeded with the construction of a Public Dock in 1929 on the Red River between Pacific and James Avenues measuring 41 feet wide by 265 feet long.

The dock was extended by 200 feet upstream in 1938. In 1953, the dock was again extended by 147 feet downstream due to a high volume of cargo being loaded and unloaded for destinations in Northern Manitoba. The cargo included mining equipment, gas, oil, food and hay for livestock to communities along the shores of Lake Winnipeg.

Between 1968 and 1970, the upstream addition was demolished.

In 1995, the Federal Government sold the Alexander Dock to the City. Since 1999, the City has been leasing the dock to the Paddlewheel Riverboats Ltd. The City completed extensive repairs to the dock between 2000 and 2004, which now has an expected life of 50 years.

ii) Amy Street Pump and Screen House

In 1923, the Amy Street Steam Plant was constructed at the foot of Rupert Avenue in proximity to the west bank of the Red River, to produce electricity to complement the Point Du Bois generating station and to provide steam heat to buildings located in the Downtown.

In 1950, the Amy Street steam plant was expanded and as a result, it required a source of coolant. The “T” shaped Pump and Screen House was built in 1951. It extended from the riverbank at the rear of the steam plant into the riverbed. The facility has three levels. Its lower basement, which is below the water level, housed six pumps arranged in two groups of three. The upper basement was known as the discharge header section. The one-storey superstructure contained the operating controls, an office, lunchroom, and switch room. Gates were used to regulate the flow of water between the river and the pump house.

The pump house is utilitarian in design except for some corbelled brickwork above its original large glass-block windows.

During the 1980s, a deck was added to the south, east and north sides of the pump house as part of the riverside walkway in Stephen Juba Park. After the steam plant ceased operations in 1990, the pump house equipment was sold and removed, and the premises were converted to office, workshop and storage space for the City’s Harbourmaster.

4.0 STANDING POLICY COMMITTEE ON DOWNTOWN DEVELOPMENT DIRECTIVE

At its meeting held on March 23, 2007, the Standing Policy Committee on Downtown Development directed the Planning, Property and Development

Department to proceed with an Invitation for EOI for the Property with a provision for the Paddlewheel Riverboats Ltd. long term occupancy of the Alexander Dock structure.

This EOI, 679-2006, is a direct result of that direction and details the City of Winnipeg's invitation to Proponents to submit Expressions of Interest for the leasing and development of 339 Waterfront Drive.

The Property is being offered for lease in an "As Is" condition and is shown as "Proposed Development Site" on Misc. Plan No. 12596/1, attached as Schedule B.

5.0 PREVIOUS STUDIES AND PREVIOUS GEO-TECHNICAL ACTIVITY

5.1 Environmental Assessments

No previous environmental investigations for the potential presence of soil contaminants have been carried out by the City of Winnipeg.

The City is unaware of any previous environmental assessments or reports for the Property.

5.2 Building Assessments

The City has obtained an audit of the building's present structural condition and identified immediate repairs. The findings of this audit are summarized in the attached Schedule C.

5.3 Previous Geo-Technical Activity

The City of Winnipeg completed an internal search and review of permits applied to and issued by its Rivers and Streams Authority regarding these premises. The results of the review are summarized in the attached Schedule D.

5.4 Cautionary Notation

The summarization of the audit and listing of geotechnical works, taken from the consultant's assessment reports and the City's records, were extracted for brevity and succinctness only.

The City advises that all Proponents should review the report in detail and records referenced herein.

The Building Condition Assessment referred to herein was prepared for use by the City of Winnipeg. This is being made available to third parties interested in submitting a Expression of Interest in response to this EOI for the Property expressly on the condition and understanding that the City, or the Consultants issuing the assessments, do not assume any responsibility for, any and all judgments, decisions, actions or inferences which any third party may deem to be derived from their interpretations or evaluations of these assessments.

The City makes no representations or warranties of any kind whatsoever regarding the contents of these assessments and reports.

6.0 CURRENT STATUS

6.1 Development Plan

The City of Winnipeg's official development plan, *Plan Winnipeg...Vision 2020*, approved by Council on December 21, 2001 designates this area as a *Downtown Policy Area*.

6.2 Alexander Dock/Harbourmaster Sites – Vision and Development Parameters

In conjunction with the formulation of this EOI, the Planning, Property and Development Department developed a Vision and Development Parameters document to guide Proponents in the preparation of Expressions of Interest for the leasing and development of the Property, a copy of which is attached as Schedule A.

The Vision and Development Parameters were developed in accordance with the approved downtown plan and design criteria for the area and in consultation with a broad cross-section of community stakeholders.

Proponents are required to prepare Expressions of Interest within the broad guidelines as specified in this document.

6.3 Relationship of The Property to the Downtown

As shown on the Misc. Plan 13041 attached as Schedule E, the Property is located in the Downtown along Waterfront Drive in close proximity to Portage and Main, the East Exchange District, the Canwest Global Baseball Stadium, the Forks, the proposed Museum of Human Rights and other cultural amenities such as the Centennial Concert Hall, the Museum of Man and Nature, Pantages Theatre, Manitoba Theatre Centre and Prairie Theatre Exchange.

The proposed leasing and development of the Property shall enhance the existing collection of attractions in the Downtown and be a destination on the Downtown Waterfront.

6.4 Relationship With Adjacent Housing Developments

During 2001, the City proceeded with the development of Waterfront Drive, a two lane riverside drive extending from the newly constructed Provencher Bridge to Higgins Avenue along the west bank of the Red River. Following the completion of Waterfront Drive, CentreVenture Development Corporation proceeded with a Expression of Interest call to invite development for housing developments fronting onto Waterfront Drive. CentreVenture Development Corporation accepted four development Expressions of Interest, which

include: Ship Street Village (8 Units), Excelsior (48 Units) Strand (15 Units) and Sky Waterfront (123 units). The first three projects are proceeding and totalling 71 new housing units at an estimated construction value of \$12.7 million.

At the time of completing this EOI, the Sky Waterfront Project has not commenced.

The successful Proponent(s) Proposal(s) for the Property should complement the adjacent condominium developments situated south and west of the Property as shown on the attached Misc. Plan 13041 and attached as Schedule E.

6.5 North Winnipeg Riverbank Parkway

The Property is located within the boundaries of the North Winnipeg Riverbank Parkway, which extends along the left bank of the Red River, from the Forks to Kildonan Park as shown on the attached Schedule A. City Council has mandated that the City should protect Riverbank Parkway Linkages wherever, and whenever, the opportunity arises to so do.

The successful Proponent shall provide for a pedestrian linkage between Stephen Juba Park and the Galt Street Pumping Station via a pathway through the Property.

6.6 Zoning and Surrounding Land Use

The site is currently zoned “R” Riverbank Sector. The permitted, conditional, accessory, and not permitted uses are listed in Downtown Zoning By-law No. 100/2004 as attached in Schedule G.

The City will not consider Expressions of Interest, which require a rezoning of the riverbank lands.

6.7 Certificate of Title Information

The Property consists of all, or portions, of the following City of Winnipeg property titles, excepting those portions of land upon which the Alexander Dock structure is situated.

<u>Certificate No.</u>	<u>Short Legal Description</u>
2032904	Parcel S, Plan 42974 WLTO
2032926	Parcel T, Plan 42974 WLTO
2032905	Parcel U, Plan 42974 WLTO
2055106	Parcel A, Plan 43293 WLTO
2055108	Parcel B, Plan 43293 WLTO
2055110	Parcel C, Plan 43293 WLTO

2055208	Sp Lot 5, Plan 33032 WLTO
1418055	Sp Lot 6, Plan 33032 WLTO
1418056	Sp Lot 7, Plan 33032 WLTO
1418035	Sp Lot 8, Plan 33032 WLTO current title excludes mines and minerals
1418034	Sp Lot 9, Plan 33032 WLTO current title excludes mines and minerals
2055205	Sp Lot 10, Plan 33032 WLTO
2055197	Lot 3, Plan 27051 WLTO
1195648	Plan 5802 WLTO

6.8 Municipal Services

The location of sub-surface utility installations within and adjacent to the subject property are shown on plans prepared by the KGS Group dated June 4, 2001.

Please note that the wastewater sewer pipe does not exist in the Alexander Avenue right-of-way, east of Waterfront Drive. Copies of the plans are attached as Schedule G.

These plans help illustrate the location of utility services i.e. wastewater sewer, land drainage, aquaduct, waterline, gas and hydro services, which traverse the site and provide service to the Property.

Public Waterline Service:

A 300 mm (PVC) watermain is located within and along Waterfront Drive as highlighted on the Plan Sk-4 and attached as Schedule G. The Proponent, will be required to pay for all costs related to the design and installation of the water service and lot line connections.

City Aquaduct:

The City Aquaduct traverses the Property between the Red River and Waterfront Drive as shown on Plan Sk-4 and attached as Schedule G. The Water and Waste Department advises that no development within 10 metres on either side of the Aquaduct will be permitted and that the City will require a 20-metre wide easement area over the Aquaduct between Waterfront Drive and the Red River for maintenance and repair purposes.

Public Wastewater Service:

There are combined sewers situated in the James, Pacific, Alexander and Galt Avenue right of ways at, or near, the intersection of the Waterfront Drive right of way. Given the location of the Property, it is likely that a lift station may be required. A new wastewater sewer or building service would have to be extended to both sites. The Proponents would be required to provide an

engineering report, prepared by a qualified municipal engineer, identifying the wastewater sewer system requirements and available capacities, including an analysis of the effects of the proposed development on the downstream sewer system. Any necessary off-site capital improvements to the sewer system to support the leasing and development of the Property shall be built at the Proponents sole cost and expense.

Land Drainage:

There are land drainage sewers located in Waterfront Drive. In addition, there is a 1500mm Combined Sewer Outfall located in the Galt Avenue right of way. The Proponent will be required to pay for all costs with the design and installation of any additional land drainage improvements.

The City will require a 10-metre wide access area over the combined sewer outfall between Waterfront Drive and the Red River for maintenance and repair purposes.

Flood Protection:

The development of lands within 350 feet of any waterway, located within the City of Winnipeg, must provide for flood control and diking as essential elements of protection. The primary line of defence (primary dike) is Waterfront Drive. This location is shown as PLD on Plan Sk-4 and attached as Schedule G.

The Property is located entirely in the Floodway Fringe Area. The Water and Waste Department reports that structures may be constructed in the area between the Primary Line of Defense and the Floodway Line in accordance with The City of Winnipeg flood proofing criteria as set out in the City of Winnipeg Charter. The Proponent will also be required to obtain a rivers and streams permit. However, no structures will be allowed within the area between the Floodway Line and the Red River all as shown on the attached Schedule H.

No construction will be allowed within the Floodway Area - between the blue line and the Red River.

Riverbank Stabilization:

In 1988, the City undertook certain riverbank stabilization improvements to the riverbank situated between James Avenue, the Pump and Screen House and the southern portion of the Alexander Docks. The City arranged for the removal of approximately 15,000 cubic metres of riverbank material.

Since 2002, additional riverbank stabilization works have been completed in conjunction with the Waterfront Drive Project. These works included the installation of rock fill columns and erosion protection works in and around

the Alexander Docks. A geotechnical-monitoring program was established pursuant to Waterway Permit No 4/2003-A as a check on riverbank performance under construction and post construction conditions.

Hydro:

As part of constructing Waterfront Drive, an underground hydro service was installed within the right of way to provide service to the Property. Proponent(s) will need to make inquiries to MB Hydro to obtain information about connecting to this service.

6.9 Eye-level photographs of the main building component and the views there from.

Eye-level photographs of the subject site show:

- the view southwest* across the Red River showing the river view of the Harbourmaster Building and portion of Dock
- the view westward* across the Red River showing the Harbourmaster Building and Alexander Dock
- the view westward across the Red River showing the Alexander Dock
- the Northern Façade and its view southward
- the Southern Façade* and its view northward
- the Eastern Façade* and its view westward
- the Western Façade* and its view eastward

* © Grand Maison Photography/www.grandmaison.mb.ca

These photographs are attached and identified within Schedule I.

6.10 Transportation/Access

Roadway Access:

The Property has direct access to Waterfront Drive, which extends from Water Avenue on the south to Higgins Avenue on the north as shown on the aerial photograph included as Schedule E.

The Property is located approximately 0.7 kilometres from the corner of Portage Avenue and Main Street, a two minute driving distance. Regional arterial access is provided to this general area:

- Southbound traffic via Main Street by making a left turn at Rupert Avenue;
- Northbound traffic via Main Street by making a right turn at Rupert Avenue;

- Eastbound traffic via Portage Avenue and Main Street by making a right turn at Pacific Avenue
- Westbound traffic via Provencher by making a right turn at Waterfront Drive; and
- Southwest bound traffic via Disraeli Freeway by making a left turn at Lily Street and proceeding south to Pacific Avenue.

The traffic volume data for the regional arterials is included as Schedule J.

6.11 Lease of Alexander Docks and Adjacent Lands

In 1999, Council approved a plan to lease a portion of the Alexander Docks and adjacent lands to Paddlewheel Riverboats Ltd. for its riverboat operations purposes. Subsequently, Council approved a plan to complete certain deck and structural repairs to the north and south halves of the Alexander Docks.

Between 1999 and 2006, the City entered into annual lease agreements with the Paddlewheel Riverboats Ltd. for the use of the Alexander Docks and adjacent lands. In 2006, Council approved a five-year lease agreement with Paddlewheel Riverboats Ltd for the use of the Alexander Docks and adjacent lands and additional leased area for riverboat operations and parking purposes as identified on the attached Misc. Plan 7806/15 – Schedule K.

The City would prefer that Paddlewheel Riverboats Ltd. continue occupying the Alexander Dock structure. However, it is prepared to terminate that part of the Lease Agreement pertaining to all other remaining portions of the Leased Premises by providing 180 days written notice to the Tenant.

In the event that a Proponent wishes to change the location of the existing vehicle and pedestrian access to the Alexander Docks (the improved portion of the former Alexander Avenue right of way extending east of Waterfront Drive), it shall provide Paddlewheel Riverboats Ltd. with an alternate means of vehicle and pedestrian access to the Alexander Dock structure, as part of any leasing and redevelopment Expression of Interest for the Property at the Proponent's sole cost and expense.

In addition, if the water and hydro service to the Alexander Docks is changed as part of an Expression of Interest, these costs shall be borne by the Proponent.

7.0 INSTRUCTIONS TO PROPONENTS

7.1 Process

The City invites Expressions of Interest for the leasing and development of the Property located on Waterfront Drive and is shown on the attached Misc. Plan No. 12596/1 - Schedule B. The leasing and development of the Property shall

be in accordance with the attached Alexander Docks Vision and Development Parameters (Schedule A).

7.2 Material and Information to be Supplied by Proponents

The Proponents should submit the following:

7.21 Background Information

- i) the names of all Principals associated with the Expression of Interest;
- ii) a list of the successful projects carried out by the Proponent, if any;
- iii) financial information about the Proponent; and
- iv) any other information, which the Proponent considers pertinent to the EOI.

7.2.2 Details of the Proposed Leasing and Site Development

The Expressions of Interest should include the proposed terms of lease inclusive of rent, term, other financial terms, conditions, if any, related to the development of the Property, the proposed land uses, an outline of the development timing, and any other relevant terms or assumptions.

It should also include a full and detailed description/explanation of the intended/envisioned Expression of Interest outlining the purpose, use and target market and be submitted together with copies of maps, plans, and drawings, for consideration by the evaluation committee

. The description should include but not be limited to the following:

- a detailed description of the intended use(s) to be established
- a description of the physical building, site and landscaping improvements to be constructed, including considerations being given to improving accessibility for handicapped persons and also indicate the estimated capital cost and phasing of implementation
- a description of services to be provided from this site including information relative to hours of operation, estimated fees and charges as may be applicable
- a description of the parking facilities to be provided and comment on traffic generation
- if a Proponent should require access to the Red River as part of its operations, then the Proponent is required to describe the intended use of the City-owned riverbank and the proposed

terms of occupation, all recognizing that the City's property is intended to be subject to the continued use by the public

- the Proponent should also indicate how it intends to accommodate the Riverbank Parkway Linkage on, and across the Property
- Proponents are required to provide a description of any similar ventures or developments created, owned or operated by the Proponent or members representing the Proponent, and verification of same by the inclusion of references
- Proponents are advised to provide all of the above information and commentary in such a fashion so as to enable the evaluation committee to evaluate the Expression of Interest.

7.3 General Conditions of EOI

7.3.1 Submission of EOI's

EOI's shall be submitted in a sealed envelope marked Alexander Dock and Harbourmaster Building Site EOI #679 - 2006, addressed and delivered to:

Materials Management
City of Winnipeg
Main Floor, 185 King Street
Winnipeg, Manitoba
R3B 1J1

Sealed EOI's will be received up to the deadline of:

12:00 Noon, Winnipeg Time, on May 28, 2007.

EOI submissions determined by the Manager of Materials to have been received later than the EOI deadline will not be accepted and will be returned upon request.

The Contract Administrator or the Manager of Materials may extend the submission deadline by issuing an addendum at any time prior to the submission deadline.

EOI's submitted by facsimile transmission (fax) or internet electronic mail (e-mail) will not be accepted.

EOI's will not be opened publicly.

The City may at any time prior to the submission deadline, issue addenda correcting errors, discrepancies or omissions in the Invitation for Expressions of Interest, or clarifying the meaning or intent of any provisions therein.

Addenda will be available on the Bid Opportunity page at the City of Winnipeg, Corporate Finance, Materials Management Branch internet site at <http://www.Winnipeg.ca/matmgt/bidopp.asp>

The Proponent is responsible for ensuring that it has received all addenda and is advised to check the Materials Management Branch internet site for addenda shortly before the submission deadline.

7.3.2 Expression of Interests on all or Part of the Lands

The City will only consider Expression of Interests for Site “A” or Site “B” or both sites. The City, in its sole discretion, may choose to award as a whole or by site.

7.3.3 Evaluation

The City shall evaluate each Expression of Interest on its own merit. Price alone will not be the sole determining factor that the City considers in the evaluation of each and every Expression of Interest.

Proposed rent, tax revenue, compatibility with surrounding uses, overall development layout, building design and quality, and compliance with the Alexander Docks Vision and Development Parameters attached as Schedule A, operational and strategic merit to the City will be critical factors within the evaluation.

7.3.4 Right to Reject

The City reserves the right to reject all or any EOI.

7.3.5 Caveat Emptor

The City makes no representations or warranty with respect to the quality, condition or sufficiency of the Property.

The subject City-owned properties will be leased on an “As Is, Where Is” basis, subject to the conditions as outlined within this E.O.I.

7.3.6 Phase I – EOI Review

The Proponents are advised to present their best Expression of Interest submission.

The Planning, Property and Development Department will review all Expression of Interests on the basis outlined in 7.3.3.

Compliance with the Alexander Docks Vision and Development Parameters will constitute a major component of the evaluation process.

If after this initial review, the Department determines that all of the Expression of Interests to be unacceptable, the Proponents will be notified and no further discussions will be held.

Upon completion of the initial Phase I review, the Department will short-list the most advantageous Expressions of Interest and then proceed to the Phase II detailed Expression of Interest Solicitation.

7.3.7 Phase II – Detailed Expression of Interest Solicitation

The Department will notify all Proponents of their status within the short listed process, and will invite the short listed Proponents to submit additional, or clarifying details, regarding their submission(s).

7.3.8 Phase III - Negotiations

The City will only negotiate with the Proponents submitting, in the City's opinion, the most advantageous Expression of Interest. The Department will enter into detailed negotiations with one or more short listed Proponents. The Expression of Interest(s) will be finalized for submission to, and consideration by, the Standing Policy Committee on Downtown Development.

The Council of the City of Winnipeg, or its designate, is the final authority for the approval of terms for a lease agreement.

7.3.9 No Contract

The Expression of Interest is an inquiry only. By responding to this EOI and participating in the process as outlined in this document, Proponents expressly understand and agree that no contract of any sort is implied or formed under, or arises from this EOI and that no legal obligations between parties has, or will be, pre-determined. The City will have no obligation to enter into negotiations or a contract with any Proponent as a result of this Expression of Interest.

7.3.10 Confidentiality

Information provided to a Proponent by the City, or by a Proponent to the City, or acquired by any party by way of further enquiries or through investigation, is strictly confidential. Such information shall not be used or disclosed in any way except as may be required for the preparation and evaluation of EOI submissions without the prior written authorization of the City, or of the Proponent.

The Proponent shall not make any statement of fact or opinion regarding any aspect of the EOI to the media or any member of the public without the prior written authorization of the Director of the Planning, Property and Development Department.

Disclosure of a successful submission by a Proponent is within the sole discretion of the Council of the City of Winnipeg, or its Designated Authority. The City may be obligated to disclose the final terms of lease pursuant to FIPPA and its internal policies and procedures.

7.3.11 No Commissions Payable

There will be no commissions payable by the City.

7.3.12 Proponent Costs and Expenses

Proponents are solely responsible for their own costs and expenses in preparing and submitting an EOI, submission and participating in the EOI including the provision of any additional information and or attendance at meetings.

7.3.13 Contact Person

Mr. W. Bollman

City of Winnipeg
Planning, Property and Development Department
2nd Floor – 65 Garry Street
Winnipeg, MB R3C 4K4

Phone (204) 986-5056

Fax: (204) 944-8476

List of Schedules

Schedule A	Alexander Docks Vision and Development Parameters
Schedule B	Sites for Leasing Misc. Plan 12596/1
Schedule C	Building Audit
Schedule D	Geotechnical Activity
Schedule E	Downtown Context Misc. Plan 13041
Schedule F	Downtown Zoning By-law 100/2004
Schedule G	Sub-surface utilities
Schedule H	Floodproofing
Schedule I	Eye-level photographs
Schedule J	Traffic data
Schedule K	Identification of Leased Lands to Paddlewheel Riverboats Ltd.

Schedule "A"

Alexander Docks Vision and Development Parameters



Alexander Docks

V i s i o n & D e v e l o p m e n t P a r a m e t e r s

P l a n n i n g , P r o p e r t y & D e v e l o p m e n t
D e p a r t m e n t

February 5, 2007

ALEXANDER DOCKS

VISION & DEVELOPMENT PARAMETERS

April 2005

PART 1 – VISION

preamble & context

This document has been prepared to assist future Requests for Expression of Interests or Expressions of Interest for developments at the Alexander Docks and Intake Structure properties. The document incorporates current Council policy commitments regarding these specific properties and broader riverbank property acquisition, use, and management policies (PART 1). The document examines the variety of site constraints (chiefly infrastructure locations and magnitude) and opportunities (PART 2). An outline of the development review process is also provided (PART 3).

These City-owned properties are located on the east side of Waterfront Drive, between James Avenue and Galt Avenue, adjacent to the Red River. The properties are a portion of the North Winnipeg Parkway, that extends from The Forks to Kildonan Park along the west bank of the Red River. To the west, across Waterfront Drive, are the James Avenue Pumping Station and the now vacant site of the former Amy Street Steam Plant. Nearby is the Exchange District National Historic Site, a former warehouse-industrial and trade and commerce area now emerging as a mixed-use neighbourhood with a significant concentration of heritage buildings. Predominantly residential infill developments along the west side of Waterfront Drive have taken place.

This site has historical significance to the settlement of Winnipeg, to its evolution as a major North American trade centre and to the inland fishery. Use of this site changed dramatically with the construction of the Amy Street Steam Plant across the railway tracks in the 1920s, the related Pump and Screen House (aka Intake Structure) in the 1950s and the initial Alexander Dock.

6.0



6.0 *View from Red River – note former Amy Steam Plant*

The steam plant was initiated to provide City Hydro with standby power. Subsequently, it provided steam heat to much of Central Winnipeg. A brick-clad utilitarian building, the Intake Structure brought water from the river into the steam plant. The Alexander Docks have been expanded, altered, rebuilt, and stabilized since initially constructed by the federal government. During their heyday in the 1950s, Lake Winnipeg catch was unloaded at the docks and lake freighters transported supplies from the docks to Northern

Manitoba and Lake Winnipeg communities. More recently, the docks have been used as public viewpoint, recreational angling spot, concert stage, and river tourboat staging area.



vision

Council is committed to the acquisition of riverbank properties for the use and enjoyment of Winnipeggers (and visitors). This commitment has been ongoing for decades and continues to be reflected in Council policies (e.g. *Plan Winnipeg: 2020 Vision* and *CentrePlan*) and in acquisition, improvement, operations, and maintenance expenditures. Council riverbank policies and expenditures apply to the whole of Winnipeg (and beyond, in terms of partnerships in initiatives such as the Agreement for Recreation and Conservation and Rivers West). More specific policies and expenditures are targeted towards Downtown Winnipeg and even more directly towards the Alexander Docks vicinity.

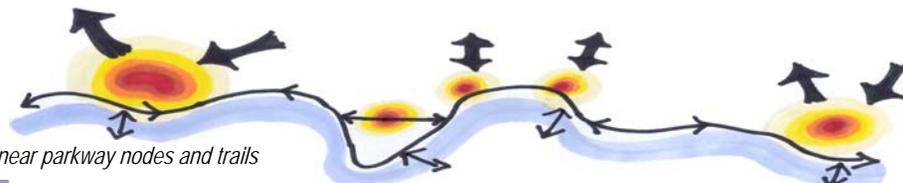
Council riverbank policies include:

- *Plan Winnipeg: 2020 Vision;*
- *CentrePlan: Vision & Strategies;*
- *CentrePlan: Development Framework;*
- *Downtown Winnipeg Urban Design Guidelines;*
- *Priorization of Development for a City-wide Riverbank Parkway System;*
- *Ad Hoc Committee on Riverbank Acquisition, Policy and Priorities, Management, and Land Use.*

Recent riverbank expenditures in this vicinity include:

- *Alexander Docks riverbank stability works;*
- *Alexander Docks structural and deck improvements;*
- *Waterfront Drive construction and primary line of defense improvements;*
- *Stephen Juba Park and North Winnipeg Parkway improvements.*

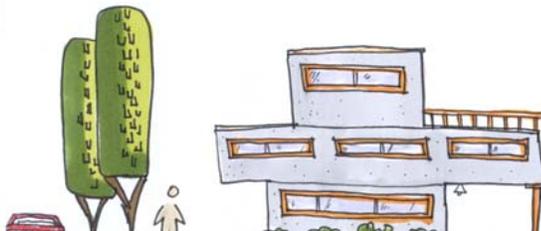
Together, these policies and expenditures comprise a vision for the use and development of the Alexander Docks / Intake Structure properties. The vision foresees an interconnected linear parkway system, with public access along the riverbanks, integrated with the adjacent neighbourhoods via comfortable streets linked to the rivers at a number of activity nodes. These nodes have many forms and encourage public gathering at a variety of intensities. The Alexander Docks site is envisioned as a key focal point for inviting visitors into the Exchange District and connecting Exchange District denizens to the North Winnipeg Parkway system.



7.0 *Linear parkway nodes and trails*



8.0 *Parkway activity node examples*



PART 2 – DEVELOPMENT PARAMETERS constraints

In total, the Alexander Docks / Intake Structure properties are approximately 12,120 square metres or 3.0 acres. See Map 1.

Significant portions of these properties either cannot be built upon due to proximity to the Red River or are constrained by the location of substantial underground utilities. Lands located within the Floodway Area cannot be built upon; lands located within the Floodway Fringe Area should meet flood-proofing criteria and should demonstrate that buildings and improvements do not negatively impact on bank stability, impede flow, or in other ways harm the waterway. See Map 2.



10.0 *Spring flooding at Alexander Docks/Intake Structure Site*

Underground utilities traverse these properties at a number of locations. Substantial infrastructure includes the aqueduct and a combined sewer line is located north of the properties. Hydro transmission lines have been relocated underground (south of and parallel to the aqueduct), and the Galt gate chamber building, which is to be reconstructed at its current location, is north of the site. See Map 2.

opportunities

Waterfront Drive was conceived to connect this portion of the Red River to the Exchange District, the downtown, and the rest of Winnipeg. Its completion in 2003 provides direct access to the Alexander Docks properties. In addition, new developments are underway west of Waterfront Drive and improvements have been made to the docks, Stephen Juba Park, and the North Winnipeg Parkway on its east side. The Alexander Docks properties are poised to fulfill their role as a key activity node along the North Winnipeg Parkway, where the Red River meets the Exchange District.



12.0 South view of the Intake Structure



11.0 Recent dock repairs

Site areas are approximately:

Development Site "A":

- site area – 6,520 square metres (1.6 acres)

Development Site "B":

- site area – 5,600 square metres (1.4 acres)

Each of the sites has vehicular access to Waterfront Drive. Currently, approximately 3500 motor vehicles travel this portion of Waterfront Drive daily. Local water service is available in the Waterfront Drive right-of-way. Wastewater sewer service is available in the Galt Avenue, Alexander Avenue, Pacific Avenue, and James Avenue rights-of-way west of Waterfront Drive. Due to the distance from these wastewater sewer lines and their comparative elevation, connections of the Alexander Docks properties to wastewater service will exceed typical costs. Land drainage sewer service is available in the Waterfront Drive right-of-way.

Note that the Intake Structure, located within Development Site "B", provides development area within the existing building (approximately 2,400 square feet; 223 m²) and features unique proximity to river's edge. Because it is located within the Floodway Fringe Area, Intake Structure development should meet floodproofing criteria.

Offsite, there's Downtown Spirit transit service north- and south-bound along Waterfront Drive and potentially via the water taxi service, on-street parking along Waterfront Drive, and potential shared off-street parking at several locations west of Waterfront Drive.

Built form cues

- ▣ public access—should provide for public passage along, to, and from the parkway;
- ▣ height—low profile;
- ▣ permeability—discourage long, uninterrupted walls paralleling Waterfront Drive;
- ▣ materials and details—to be viewed from 360 degrees (pavilion); quality and durability of materials consistent with *Plan Winnipeg* and *Downtown Winnipeg Urban Design Guidelines*;

- ▣ quality of the public realm—encourage public spaces and routes that are attractive, safe, uncluttered, have purpose and work effectively for all people, to the greatest extent possible, without the need for adaptation or specialized design.

Preferred built form

- ▣ one- to three-storeys;
- ▣ building entrance from both riverside and roadside;
- ▣ abbreviated wall lengths parallel to Waterfront Drive and Red River, to provide multiple views and access points to and from the river between buildings.

Land use cues

- ▣ public gathering—should provide for public activity and may provide for private social functions;
- ▣ context—uses to have relationship to the site (e.g. river, parkway, Exchange District);
- ▣ variety—mix of uses encouraged;
- ▣ programming—encourage range of uses that together utilize the location year-round.

Preferred land use mix

- ▣ variety of uses and users that have a strong relationship to the riverside and parkway locations;
- ▣ including: public parkway trails and passive spaces; angling and pleasure docking; river tourboat terminus; restaurant/banquet facility; recreation services and rentals/sales; seasonal marina; interpretive centre and viewpoint.

Zoning

- ▣ properties are zoned "Riverbank Sector" under *Downtown Winnipeg Zoning By-Law* No. 100/2004;
- ▣ accommodates a limited range of uses that are accessory to a park use, such as retail sales, personal services, restaurant, studio for artists and others, office and primarily spectator gallery.

Parking, loading, building services

- ▣ parking incidental to onsite developments is to be located within Floodway Fringe Area (above bank);
- ▣ parking to be provided in unobtrusive bundles and landscaped and screened in such a way that larger shared parking areas are separated into smaller clusters;
- ▣ larger parking areas are encouraged to be provided in shared facilities offsite;
- ▣ customer/passenger drop-off areas to be provided within Floodway Fringe Area (above bank);
- ▣ access to loading, building services, and the docks to be accommodated with minimal intrusion when within Floodway Area (below bank);
- ▣ equipment storage and maintenance operations to be located within enclosed building.

PART 3 – DEVELOPMENT REVIEW PROCESS

Following Expression of Interest selection and lease agreement with the City of Winnipeg and prior to receiving development permits, the following development attributes should be reviewed and approved:

Urban Design

- ▣ urban design review is required for all development visible from public right-of-way or river, in accordance with the *Downtown Winnipeg Urban Design Guidelines*

Zoning

- ▣ should comply with "Riverbank Sector" regulations (land use, built form, parking, loading, signs) of the *Downtown Winnipeg Zoning By-Law*

Waterways

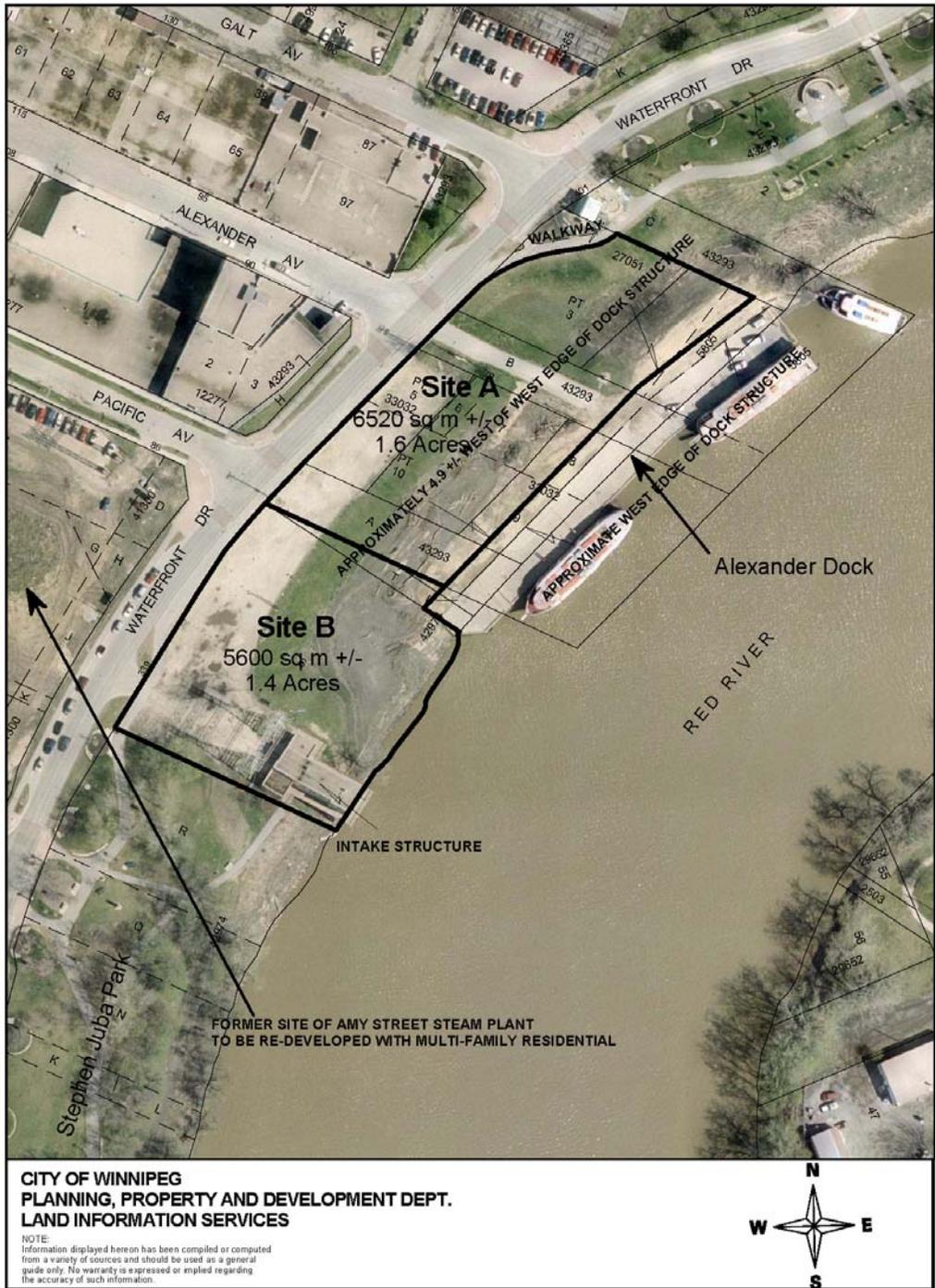
- ▣ any development within 350 feet of the Red River requires a Waterway Permit (floodproofing criteria: effects on bank stability, flow impediment, waterway operations)

Building Code

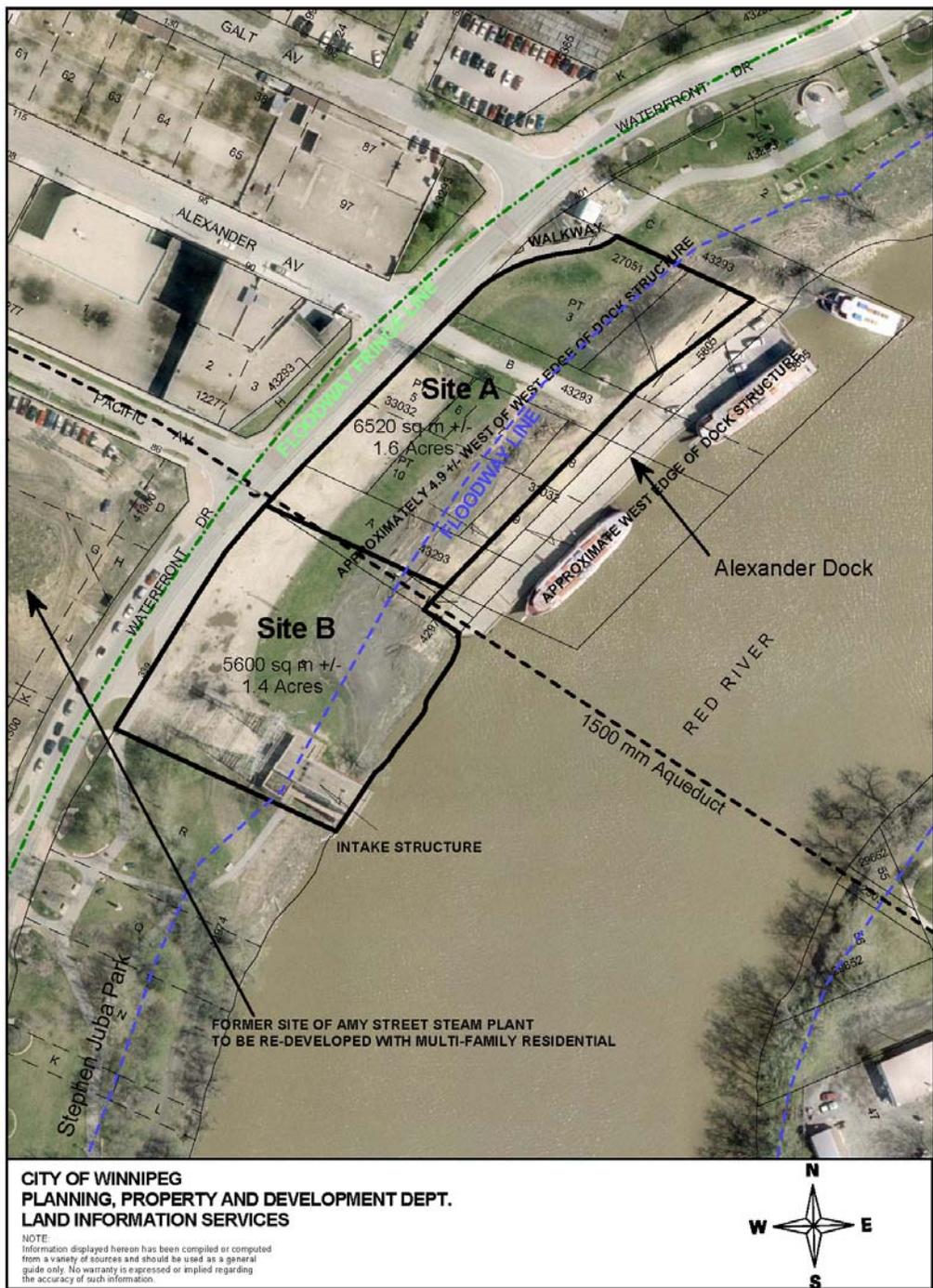
- ▣ plan examination is required to determine compliance with Winnipeg and Manitoba Building Codes

Others

- ▣ any development near water is subject to the federal Fisheries Act, in particular the Habitat Protection Provisions which address the impact of undertakings/works on fish and fish habitat. Developments and subsequent maintenance activities proposed for the Alexander Docks / Intake Structure properties will be referred to Fisheries and Oceans Canada, in accordance with the "Request for Expression of Interest Review" or the "Expression of Interest" process;
- ▣ any development placed in, on, over, under, through, or across navigable water is subject to the federal Navigable Waters Protection Act. Developments and subsequent operations proposed within the Red River waterway will be referred to Transport Canada to determine effects on navigation.
- ▣ depending on specifics of the Expression of Interest, may also require:
 - business license;
 - private approach approval—for relocated motor vehicle access;
 - MLCC approval.



Map 1

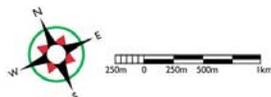


Map 2



North Winnipeg Parkway

- parkway route
- trailhead
- park/open space



Map 3

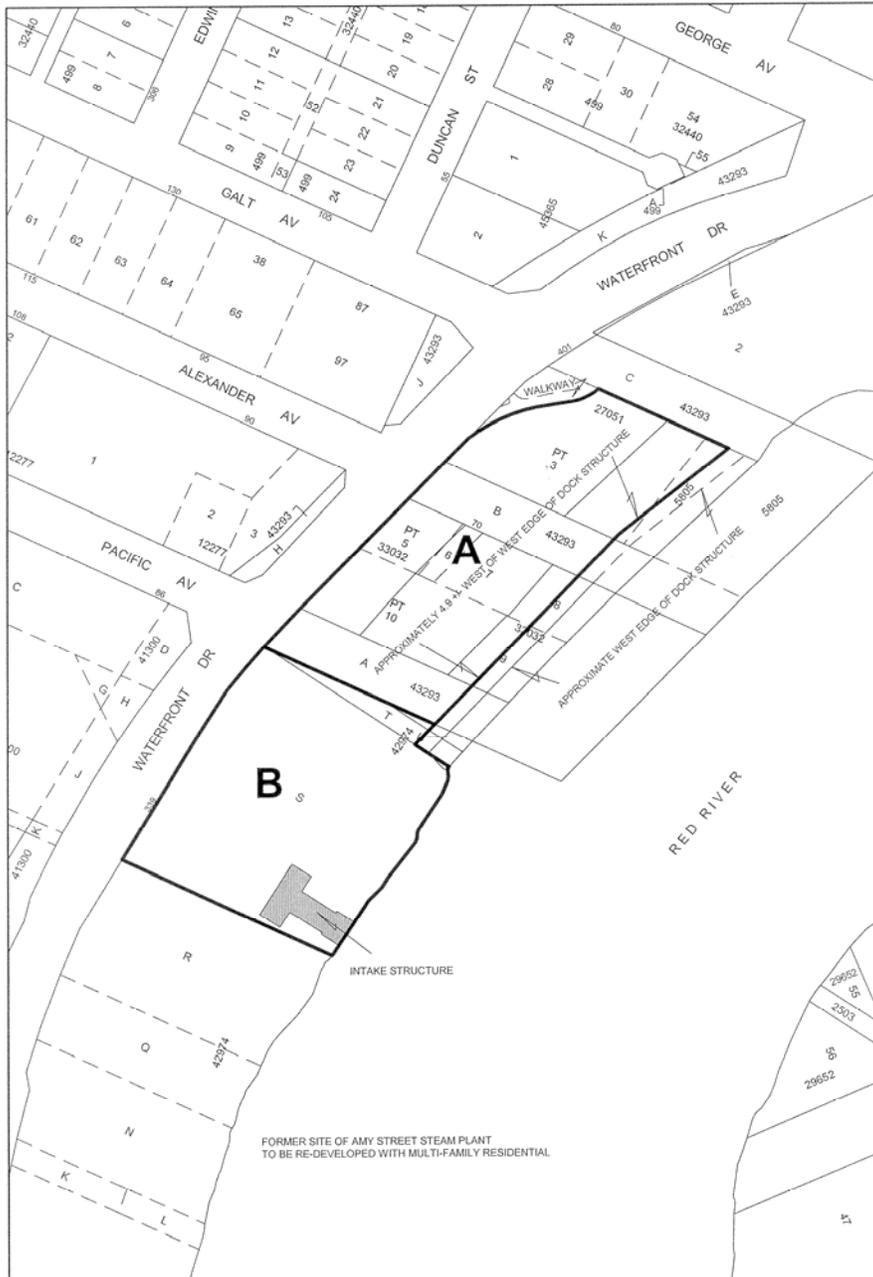
Pathway Conditions

- Type 1 - asphalt/concrete, minor grade changes
- - - Type 2 - compacted gravel, minor grade changes
- · · · · Type 3 - rough surface/ uneven grade
- · · · · Type 4 - steep slope



Schedule "B"

Sites for Leasing Misc. Plan 12596/1



 Proposed Development Site
 Site A = 6520 sq.m. +/-
 Site B = 5600 sq.m. +/-



METRIC

Date: 2006-DEC-6
 District: LSWK
 File: 38 / 752 V7
 Scale: 1:1500

NOTE:
 Information displayed hereon has been compiled or computed from a variety of sources
 and should be used as a general guide only.
 No warranty is expressed or implied regarding the accuracy of such information.

CITY OF WINNIPEG
PLANNING, PROPERTY AND DEVELOPMENT DEPT.
LAND INFORMATION & MAPPING SERVICES

MISC. PLAN NO. 12596 / 1

Schedule "C"

Building Audit

CROSIER
KILGOUR &
PARTNERS
LTD.

Consulting
Structural Engineers

1.0 INTRODUCTION

A structural review of the Harbour Master Building, located on Waterfront Drive was completed in October of 2005. The primary purpose of the review was to provide an opinion on the building's present structural condition and identify any immediate repair requirements as well as long-term considerations necessary for the continued use of the building. The scope of the investigation consisted of a visual review only, combined with hammer soundings on representative structural components. No invasive inspection recesses were completed.

The following report summarizes our observations, comments, and recommendations. Budget construction costs for recommendations are presented while photographs of significant findings are appended for clarity.

2.0 STRUCTURAL SYSTEM

Copies of original building structural drawings were provided by the City of Winnipeg. According to the drawings, the Harbour Master Building was designed and built in 1950. The building has a gross area of approximately 3,000 square feet (280 square meters) and is essentially a single storey building. Two basement levels are present in the central section of the building, 20 feet and 36 feet, respectively below street level.

The building utilizes cast-in-place concrete foundation walls supported on a shallow foundation system; piers set onto glacial till. The concrete foundation walls project up from the river bank to the street level. The superstructure walls consist of multi-wythe clay brick which carries the gravity roof load and lateral wind loads.

The roof system consists of gypsum based panels, approximately four inches thick. The panels contain embedded reinforcing steel bars and span between steel purlins, spaced approximately five feet on-centre. Support for the steel frame is provided by the clay brick walls.

A structural steel/wood deck walkway has been attached to the building and is not original to the structure.

3.0 SUMMARY OF FINDINGS

The following summarizes in point form our significant observations. Photographs of significant findings are provided in Appendix A.

- Photograph 1 shows the general front elevation. In general, the concrete foundation walls were observed to be in good condition. Minor cracking, associated with differential foundation movement is present at the front of the building, as shown in Photograph 2. Differential movement between the north and south blocks is clearly evident.

- The east or river front concrete superstructure was also generally in good condition. However, major spalling and delamination was present in the concrete at the water line. Photograph 3 shows the large section of separation in the concrete, presumably from freeze-thaw deterioration in the concrete. The extent of damage does not appear to have adversely affected the structural integrity, however.
- The masonry walls along the central section were generally in good condition. Mortar joint deterioration was present in numerous areas. Photograph 4 shows the masonry joint deterioration at the front or west face. Photograph 5 shows the parapet mortar joint deterioration.
- A significant structural concern exists however, regarding the integrity of the east and west block parapets. Photograph 6 shows the bulging masonry. Photograph 7 shows the corresponding area at the roof. The open roof membrane has allowed water to penetrate the wall assembly. Re-building of the parapet will be required.
- Water is penetrating into the interior where the parapet is cracked and separated. In addition, the coping is displaced, as shown in Photograph 8.
- The cementitious parging applied over the window infill areas was observed to be delaminating in several areas. Photograph 9 shows a typical area of deterioration. Moisture cascading down the building and from wind-driven rain is likely responsible for the delamination. Although not structurally significant, removal of the loose material is required because of its proximity to the sidewalk around the perimeter of the building.
- Regarding the interior, extensive delamination, cracking and spalling in the gypsum roof panels has been previously identified. Replacement of the roof system is required if the building is to be used in any capacity.
- The steel framing which supports the gypsum panels was observed to be generally in good condition. Their re-use in a new roof structural system should be feasible.
- The interior concrete structural floor slabs, beams, and columns were all observed to be in good condition. Heavy corrosion on the metal grating was present on the lower level, likely from long-term contact with moisture. Considerable loss of cross-section has occurred in the lower level. Thus, if the lower level is to be utilized in any capacity, such as storage, replacement of the grating should be considered.
- Photograph 10 shows the front entrance slab; extensive freeze-thaw deterioration to the surface has occurred. Although not structurally significant at the present, the deep erosion presents a tripping hazard and repairs should be considered if the building is going to be utilized.

4.0 ANALYSIS AND RECOMMENDATIONS

The visual review of the Harbour Master Building confirmed that the overall structural components are generally in good condition with exception to the gypsum roof panels. The concrete frame and majority of the multi-wythe masonry walls are also in good condition with only localized repairs being presently required.

There is a significant structural concern with respect to the condition of the gypsum roof panels. We cannot rule out localized collapse could occur when exposed to the present design snow load, particularly due to drifting effects between the north/south wings and the central section. Thus, based on the condition of the gypsum roof panels, we recommend that the building not be used for any purpose until the roof panels are shored or replaced.

If the building is to be considered for use, there are structural repairs that are warranted for completion concurrently with the roof panel replacement. Specifically, the north and south masonry parapets require replacement due to significant displacement.

The loose parging on the window infill areas should also be removed to prevent potential injury to persons on the sidewalk which surrounds the perimeter of the building.

If long-term use of the facility is to be considered, other structural repairs which will be required include localized repointing and shotcrete repairs to the east concrete platform.

Thus, if the building is to be utilized in any capacity, the following repairs are required prior to occupation:

1. Replace roof panels and roofing membrane.
2. Rebuild north and south wing parapets.
3. Remove loose parging on window infill areas.

If long-term use of the building is to be contemplated, the following repairs are recommended although not necessarily mandatory.

1. Re-point loose/deteriorated mortar joints to prevent water penetration into the building.
2. Repair front entrance slab.
3. Remove loose concrete and complete shotcrete repairs to east concrete platform.



5.0 ESTIMATE OF PROBABLE CONSTRUCTION COSTS

Accurate estimate of construction repair costs for buildings is difficult to provide because of the inherent number of hidden variables associated with an existing building. However, based on our visual review of the building and our experience with working on similar vintage buildings, the following table summarizes the estimated construction costs for the repairs listed above.

Item	Cost
Immediate Requirements	
1. Remove and replace gypsum panel roof system with metal deck, drywall and built-up roof membrane.	\$70,000.00
2. Replace north and south wing masonry parapets.	\$35,000.00
3. Remove loose parging at window infill areas.	\$2,000.00
4. Mobilization, set-up, site protection; 15 Percent.	\$15,000.00
Total	\$122,000.00
Long-Term Considerations	
1. Re-point loose/open mortar joints.	\$15,000.00
2. Repair front entrance slab.	\$3,000.00
3. Shotcrete repairs to east dock.	\$15,000.00
Total	\$33,000.00

Please be advised that the repair costs listed are for construction only and do not include contingencies or professional fees for design drawings, specifications, tendering, inspections and project administration. In addition, a contingency allowance should be carried to help cover unforeseen items should they arise.

6.0 SUMMARY AND CONCLUSION

The visual review of the Harbour Master Building confirmed that in general, the primary structural components are in good condition with exception to the roof panels. Due to the poor condition of the gypsum roof panels, replacement or at a minimum, shoring of the roof system is required if the building is to be used in any capacity. Thus, if no repairs to the roof system are to be completed, the building should be locked and entry restricted.

Replacement of the parapets on the north and south wings is also required because of significant displacement from moisture and freeze-thaw cycling.



**CROSIER
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Consulting
Structural Engineers

**THE CITY OF WINNIPEG
HARBOUR MASTER BUILDING
BUILDING STRUCTURE AUDIT**

**File No. 2005-867
October, 2005
Page 5 of 5**

Immediate repair requirements are estimated to be \$122,000.00 if the building is to be used in any capacity. Additional, albeit discretionary, repairs are referenced if the building is to be utilized for the long-term, say greater than 10 years.

We trust the above report provides the information you require. If however, you have any questions or require clarification, please contact the undersigned at your convenience.

John A. Wells, M. Sc., P. Eng.



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Schedule "D"
Geotechnical Activity

The City of Winnipeg
Waterway Permit

BOOK

PERMIT NO: 71/2001

Under and by virtue of the powers vested in it under the provisions of The City of Winnipeg Act, The City of Winnipeg hereby authorizes (subject to the conditions hereinafter specified):

APPLICANT: KGS Group, 3rd Floor-865 Waverley Street, Winnipeg, Manitoba
R3T 5P4 APPLICANT CONTINUED...

to undertake work on the following described lands, contained within the regulated area of The City of Winnipeg Waterway By-law namely:

LEGAL DESCRIPTION: Plan 4776

MUNICIPAL LOCATION: and commonly known as:
Waterfront Drive (Lombard Avenue to James Avenue)

SUBJECT: the following work namely:
Construct approximately 420 lineal metres of roadway, to be commonly known as Waterfront Drive, Phase 1 Construction, adjacent the west bank of the Red River between Lombard Avenue and James Avenue. Works include installation of underground services and renewal of the City's existing sewer and water-main system.
Proposed roadway to serve as the City's primary line of flood defence with design elevation typically at 230.2 metres.

SUBJECT CONTINUED...

- CONDITIONS:
1. Subject to Tender Package and drawings of The City of Winnipeg Tender No. 654-2001 "Waterfront Drive - Phase 1: Lombard Avenue to James Avenue".
 2. Subject to the Engineer's report prepared by KGS Group "Waterfront Drive, Riverbank Stability Report for Phase 1 Construction (Lombard Avenue to James Avenue Pumping Station) dated August 2, 2001, and, in particular, the following:

CONDITIONS CONTINUED...

EXPIRATION: This permit shall expire and the right of the owner under the permit shall terminate if the work authorized by the permit is not commenced within one (1) year from the date of issuance of the permit and reasonably continued without interruption thereafter.

IMPORTANT NOTICE: The permit holder is reminded that this permit is issued in pursuance of The City of Winnipeg Waterway By-law only, and does not constitute authority for any particular use of the land designated therein which may be within the regulatory powers of the Government of Canada, the Government of Manitoba, or The City of Winnipeg within which the land is situate.

DATED August 14, 2001 in The City of Winnipeg, in The Province of Manitoba this 14 day of August A.D. 2001

Estimated Cost: \$1,400,000
Permit Fee : \$2751.90
Receipt Number: 0

(Paid JE)


Designated Employee, The City of Winnipeg

APPLICANT CONTINUED...

(on behalf of The City of Winnipeg, Public Works Department,
2000 Portage Avenue, Winnipeg, Manitoba, R3J 0K1)

SUBJECT CONTINUED...

All of the above as per Tender Package and drawings of The City of Winnipeg Tender No. 654-2001, "Waterfront Drive, Phase 1, Lombard Avenue to James Avenue" submitted and date stamped August 7, 2001.

Engineer's report by KGS Group, "Waterfront Drive, Riverbank Stability Report for Phase 1 Construction (Lombard Avenue to James Avenue Pumping Station)" dated August 2, 2001, submitted in support of application.

The proposed roadway will be located, at closest, approximately 47 metres from the Red River.

CONDITIONS CONTINUED....

- a) Pursuant to a Waterway Permit, riprap erosion protection works are to be implemented along the river's edge between Lombard and James Avenues during the winter of 2001-2002.

The City of Winnipeg Waterway Permit

PERMIT NO: 3/2002

Under and by virtue of the powers vested in it under the provisions of The City of Winnipeg Act, The City of Winnipeg hereby authorizes (subject to the conditions hereinafter specified):

APPLICANT:

KGS Group, Attention: Rob Kenyon, P. Eng., 3rd Floor, 865 Waverley Street, Winnipeg, Manitoba R3T 5P4 APPLICANT CONTINUED...
to undertake work on the following described lands, contained within the regulated area of The City of Winnipeg Waterway By-law namely:

LEGAL
DESCRIPTION:

MUNICIPAL
LOCATION:

and commonly known as:

Waterfront Drive (Lombard Avenue to May Street)

SUBJECT:

the following work namely:

Perform the following temporary access works to facilitate Winter 2001-2002 riverbank stabilization construction along the west bank of the Red River between Lombard Avenue and May Street for the proposed Waterfront Drive Project.

Lombard Avenue to the James Avenue Pumping Station

1. Construct a temporary access ramp adjacent the upstream side of the James Avenue Pumping Station by the excavation and removal of material between the uplands and the lower bank area.

CONDITIONS:

SUBJECT CONTINUED...

1. Subject to the Engineer's report by KGS Group, "Waterfront Drive (Lombard Avenue to Heaton Avenue) Phase 2 - Riverbank Protection Works (City of Winnipeg Tender No. 904-2001) Site Access Waterway Application," dated January 14, 2002 and, in particular, the construction methodology and sequencing identified therein.

2. Subject to the City of Winnipeg Tender Package No. 904-2001, "Waterfront Drive (Lombard Avenue to Heaton Avenue) Phase 2 - Riverbank Remediation Works," including Addendum Nos. 1, 2, 3 and 4.

CONDITIONS CONTINUED...

EXPIRATION:

This permit shall expire and the right of the owner under the permit shall terminate if the work authorized by the permit is not commenced within one (1) year from the date of issuance of the permit and reasonably continued without interruption thereafter.

IMPORTANT
NOTICE:

The permit holder is reminded that this permit is issued in pursuance of The City of Winnipeg Waterway By-law only, and does not constitute authority for any particular use of the land designated therein which may be within the regulatory powers of the Government of Canada, the Government of Manitoba, or The City of Winnipeg within which the land is situate.

DATED in The City of Winnipeg, in The Province of Manitoba this 17 day of January, A.D. 2002

Estimated Cost: \$100,000.00

Permit Fee : \$258.90

Receipt Number: 7201

CW 162 99 11


Designated Employee, The City of Winnipeg

APPLICANT CONTINUED...

(on behalf of Chabot Enterprises Ltd., Box 17, Group 545, RR#5, Winnipeg, Manitoba R2C 2Z2, under contract with City of Winnipeg, Public Works Department, Attention: Fred Corey, P. Eng. 106 - 1155 Pacific Avenue, Winnipeg, Manitoba R3E 3P1)

SUBJECT CONTINUED...

2. Construct a temporary access roadway, approximately 5.0 metres wide, along the lower bank area below the regulated summer water level between McDermot Avenue and James Avenue. Lower access roadway to be constructed by minor regrading and/or placement of rockfill. Works also include construction of an at grade roadway with turnaround along mid-bank area downstream of Bannatyne Avenue to James Avenue.

Alexander Avenue to May Street

1. Construct three (3) temporary access ramps from proposed Waterfront Drive to the river edge as follows: Alexander Avenue along lower bank to Galt Avenue, at George Avenue and at May Street. Access ramps to be constructed at grade except for riverbank excavation works to facilitate ramp off George Avenue.
2. Construct a temporary access roadway, approximately 5.0 metres wide, along the lower slope area between Galt Avenue to May Street by minor regrading above winter ice level.

All of the above as per Engineer's report and drawings by KGS Group, "Waterfront Drive (Lombard Avenue to Heaton Avenue) Phase 2 - Riverbank Protection Works (City of Winnipeg Tender No. 904-2001) Site Access Waterway Application," dated January 14, 2002 submitted in support of application.

CONDITIONS CONTINUED...

3. The subject works are to be performed under the full-time supervision of a qualified Geotechnical Engineer hereafter referred to as the Engineer.
4. Excavated material from site preparation and construction is to be removed directly offsite.
5. No site access or works are allowed in the Alexander Dock area, particularly, between Alexander Avenue and the James Avenue Pumping Station. (Note: A structural assessment of the Dock must be completed and geotechnical monitoring performed with Engineer's report on same submitted to the satisfaction of Waterways in support of any proposed works in this area.)
6. Loaded truck traffic is restricted from using the access ramp off Alexander Avenue to Galt Avenue.
7. Access roadways are not to exceed 5 metres in width unless the requisite approval in the prescribed form has been obtained from Waterways.
8. Bank stability conditions particularly in the area in/around the Alexander Dock area are to be monitored over the duration of construction. (Note: Monitoring reports are to be submitted to the satisfaction of Waterways on a regular basis.)

and

Terms and Conditions as set out in Schedule "A" which is attached hereto and forms part of Waterway Permit No. 3/2002.

SCHEDULE "A"

(Attached to and forms part of Waterway Permit No. 3/2002)

CONDITIONS CONTINUED...

9. Temporary work areas and any bank area damaged as a result of access or construction activity are to be restored to a condition at least compatible to that which existed prior to construction.
10. Storage or stockpiling of materials or construction supplies is not permitted on or in close proximity to the riverbank. (NOTE: Any storage, stockpile or construction staging areas are to be approved by the Engineer and located in areas so as not to adversely impact on bank stability.)
11. Vegetation is to be promoted in all restored and disturbed areas as soon as weather conditions permit.
12. Erosion control measures such as silt fences or erosion control mats are to be in place over the duration of construction and thereafter until such time as vegetation cover has been effectively introduced in all affected areas.

NOTE: This Permit is only issued in the context of the subject works. Waterway Permit(s) must be obtained prior to implementation of any other works contemplated for the Waterfront Drive Project including temporary ramp/roadway access works between James Avenue Pumping Station and Alexander Avenue and construction of permanent riverbank stabilization works between Lombard Avenue and May Street.

The City of Winnipeg Waterway Permit

PERMIT NO: 4/2002

Under and by virtue of the powers vested in it under the provisions of The City of Winnipeg Act, The City of Winnipeg hereby authorizes (subject to the conditions hereinafter specified):

APPLICANT:

KGS Group, Attention: Rob Kenyon, P. Eng., 3rd Floor, 865 Waverley Street, Winnipeg, Manitoba R3T 5P4 APPLICANT CONTINUED...
to undertake work on the following described lands, contained within the regulated area of The City of Winnipeg Waterway By-law namely:

LEGAL DESCRIPTION:

MUNICIPAL LOCATION:

and commonly known as:

Waterfront Drive (from Lombard Avenue to Heaton Avenue)

SUBJECT:

the following work namely:

Perform the following riverbank stability improvement works along the west bank of the Red River between Lombard Avenue and May Street in conjunction with Waterfront Drive Roadway Project:

Lombard Avenue to James Avenue Pumping Station

1. Deposit approximately 3,000 cubic metres of rockfill to a maximum depth of approximately 0.6 metres as an erosion control blanket along approximately 250 lineal metres of shoreline.

CONDITIONS:

SUBJECT CONTINUED...

1. Subject to the Engineer's report by KGS Group "Waterfront Drive Riverbank Stability Report for Phase II Construction, Riverbank Protection Works, Lombard Avenue to May Street" dated December 13, 2001.
2. Subject to City of Winnipeg Tender No. 904-2001 "Waterfront Drive (Lombard Avenue to Heaton Avenue) Phase 2 - Riverbank Remediation Works."

CONDITIONS CONTINUED...

EXPIRATION:

This permit shall expire and the right of the owner under the permit shall terminate if the work authorized by the permit is not commenced within one (1) year from the date of issuance of the permit and reasonably continued without interruption thereafter.

IMPORTANT NOTICE:

The permit holder is reminded that this permit is issued in pursuance of The City of Winnipeg Waterway By-law only, and does not constitute authority for any particular use of the land designated therein which may be within the regulatory powers of the Government of Canada, the Government of Manitoba, or The City of Winnipeg within which the land is situate.

DATED 23 in The City of Winnipeg, in The Province of Manitoba this 23 day of January, A.D. 2002

Estimated Cost: \$1,400,000.00
Permit Fee : \$2,709.52
Receipt Number: 0 Paid (JE)


Designated Employee, The City of Winnipeg

APPLICANT CONTINUED...

(on behalf of the City of Winnipeg, Public Works Department,
106 - 1155 Pacific Avenue, Winnipeg, Manitoba R3E 3P1)

SUBJECT CONTINUED...

Rockfill to be deposited from the ordinary high water mark (OHWM) and extend approximately 10 metres beyond winter ice level and blend into existing erosion protection works downstream of CNR bridge and adjacent to the existing CAR-RAC Bannatyne Dock.

Downstream of Alexander Dock to May Street

1. Construct fifty-seven (57) rockfill columns in one (1) row, each 2.7 metres diameter, along approximately 250 lineal metres of shoreline between the downstream end of Alexander Dock to May Street.

Rockfill columns to be constructed along lower bank area at approximate elevation 226 metres and keyed a minimum of 0.3 metres into the underlying glacial till.

2. Construct a granular shear key along approximately 250 lineal metres of shoreline on the riverside of the proposed rockfill columns at regulated summer water level. Shear key construction includes the excavation of a 5-metre wide trench to a minimum depth of 0.3 metres into the basal till and infill of same with approximately 7,200 cubic metres of compacted rockfill.
3. Deposit approximately 3,000 cubic metres of rockfill to a maximum depth of approximately 0.6 metres as an erosion control blanket along approximately 250 lineal metres of shoreline.

Rockfill to be deposited from the OHWM and extend approximately 10 metres beyond winter ice level and blend into existing shoreline downstream of May Street at the Red River.

Galt Avenue Outfall

Perform the following replacement works for construction of a new land drainage outfall at Galt Avenue (RR-64) outfall:

1. Install approximately seventy (70) metres of 1600 mm diameter C.S.P. land drainage sewer outfall pipe complete with barrier screen and end section. Outfall to be installed by open cut method.

All of the above as per drawings and Tender No. 904-2001, "Waterfront Drive (Lombard Avenue to Heaton Avenue) Phase 2 - Riverbank Remediation Works" submitted and date stamped December 31, 2001.

Engineer's report by KGS Group "Waterfront Drive Riverbank Stability Report for Phase II Construction, Riverbank Protection Works, Lombard Avenue to May Street" dated December 31, 2001.

CONDITIONS CONTINUED...

3. The works are to be performed under full-time supervision of the KGS Group hereafter referred to as the Engineer.

and

Terms and Conditions as set out in Schedule "A" which is attached hereto and forms part of Waterway Permit No. 4/2002.

SCHEDULE "A"

(Attached to and forms part of Waterway Permit No. 4/2002)

CONDITIONS CONTINUED...

4. The riverbank is to be monitored (visual/instrumentation) during all phases of construction with Waterways apprised of the results and interpretation of same on a regular basis.
5. Construction activity is to be confined only to the subject work area.
6. Lower bank works are to be carried out during the winter months when the ground is frozen and can sufficiently support vehicular traffic and construction activity.
7. Storage or stockpiling of materials or construction supplies is not allowed on or near the riverbank. (Note: Any storage or stockpile areas are to be approved by the Engineer and located in areas so as not to adversely impact on bank stability.)
8. All excavated material from earthwork operations (rockfill column excavation, trench shear key excavation, etc.) is to be removed directly from the site and not deposited within the regulated area under Waterway By-law No. 5888/92.
9. Any riverbank area damaged as a result of construction or vehicular access is to be restored to a condition at least equivalent to that which existed prior to construction.
10. As the methodology and sequencing of the proposed riverbank stabilization works are very important, the Contractor is to exercise due diligence in ensuring that the implementation process and controls as reviewed and approved by the Engineer are adhered.
11. Rockfill column construction between the Alexander Dock and May Street is to be completed prior to initiation of shear key construction works at regulated summer river level.
12. Under no circumstances shall the Contractor be allowed to leave any excavation open overnight. Key trench excavation shall be done concurrently with backfill/compaction operations so as there is no more than 2 metres of exposed length of excavation along the bottom at any time.
13. The works are to proceed in a timely manner so as temporary stockpiling operations are minimized.
14. The lines and grades of the proposed rockfill riprap are to provide a uniform shoreline and are to be tapered or feathered to smoothly blend with bank contours of adjacent properties so as not to adversely impact on the erosion or stability of the adjacent sections of riverbank.
15. Upon completion of the subject works, the Engineer is to submit as-built drawings including a letter to the Waterways Section, Unit 15, 20 Fort Street, Winnipeg, Manitoba R3C 4X5 certifying that all works have been completed to his satisfaction and in compliance with this Permit.

NOTE: This Permit is issued only for the items identified in the subject. Waterways approval will be required for proposed riverbank stabilization works in the Alexander Dock area including construction of rockfill columns upstream of the James Avenue Pumping Station and any subsequent upland road works between James Avenue and Heaton Avenue.

The City of Winnipeg Waterway Permit

PERMIT NO:

4/2002-A AMENDED

Under and by virtue of the powers vested in it under the provisions of The City of Winnipeg Act, The City of Winnipeg hereby authorizes (subject to the conditions hereinafter specified):

APPLICANT:

KGS Group, Attention: Rob Kenyon, P. Eng., 3rd Floor, 865 Waverley Street, Winnipeg, Manitoba R3T 5P4 APPLICANT CONTINUED...
to undertake work on the following described lands, contained within the regulated area of The City of Winnipeg Waterway By-law namely:

LEGAL
DESCRIPTION:

MUNICIPAL
LOCATION:

and commonly known as:

Waterfront Drive (from Lombard Avenue to Heaton Avenue)

SUBJECT:

the following work namely:

Perform the following riverbank stability improvement works along the west bank of the Red River between Lombard Avenue and May Street in conjunction with Waterfront Drive Roadway Project:

Lombard Avenue to James Avenue Intake Structure

1. Deposit approximately 3,000 cubic metres of rockfill to a maximum depth of approximately 0.6 metres as an erosion control blanket along approximately 250 lineal metres of shoreline.

CONDITIONS:

SUBJECT CONTINUED...

1. Subject to the Engineer's report prepared by KGS Group "Waterfront Drive Riverbank Stability Report for Phase II Construction, Riverbank Protection Works, Lombard Avenue to May Street" dated December 31, 2001.
2. Subject to the Engineer's report prepared by KGS Group "Phase 2 - Riverbank Remediation Works" dated February 1, 2002 and, in particular, the sequenced construction schedule identified therein for stabilization works between the James Avenue Intake Structure and the Alexander Dock.

CONDITIONS CONTINUED...

EXPIRATION:

This permit shall expire and the right of the owner under the permit shall terminate if the work authorized by the permit is not commenced within one (1) year from the date of issuance of the permit and reasonably continued without interruption thereafter.

IMPORTANT
NOTICE:

The permit holder is reminded that this permit is issued in pursuance of The City of Winnipeg Waterway By-law only, and does not constitute authority for any particular use of the land designated therein which may be within the regulatory powers of the Government of Canada, the Government of Manitoba, or The City of Winnipeg within which the land is situate.

DATED in The City of Winnipeg, in The Province of Manitoba this7..... day of February... A.D.2002

Estimated Cost: \$1,400,000.00
Permit Fee : \$2,709.52
Receipt Number: 0 (Paid JE)


Designated Employee, The City of Winnipeg

APPLICANT CONTINUED...

(on behalf of the City of Winnipeg, Public Works Department,
106 - 1155 Pacific Avenue, Winnipeg, Manitoba R3E 3P1)

SUBJECT CONTINUED...

Rockfill to be deposited from the ordinary high water mark (OHWM) and extend approximately 10 metres beyond winter ice level and blend into existing erosion protection works downstream of CNR bridge and adjacent to the existing CAR-RAC Bannatyne Dock.

2. Construct twenty-one (21) rockfill columns in two (2) rows, each 2.7 metres diameter, along approximately 30 lineal metres of shoreline immediately upstream of James Avenue Intake Structure.

Rockfill columns to be constructed along lower bank area at approximate elevation 223.7 metres and keyed a minimum of 0.3 metres into the underlying glacial till.

James Avenue Intake Structure to downstream of Alexander Dock

1. Deposit approximately 2000 cubic metres of rockfill to a maximum depth of 0.6 metres as an erosion control blanket along approximately 200 metres of shoreline between the upstream side of James Avenue Intake Structure and the downstream end of the Alexander Dock.

Rockfill columns to be deposited from the OHWM and extend approximately 10 metres below winter ice level in the area between the upstream side of James Avenue Intake Structure and Alexander Dock.

Rockfill to be deposited on the riverside of the Alexander Dock and extend from the winter river level to approximately 8 metres into the channel area.

2. Construct fifty-five (55) rockfill columns in four (4) rows, each 2.7 metres diameter, along approximately 50 metres of shoreline between the James Avenue Intake Structure and the Alexander Dock.

Rockfill columns to be constructed along lower bank area at approximate elevation 223.7 metres diameter and keyed a minimum of 0.3 metres into the underlying till.

3. Construct one hundred and nine (109) rockfill columns in three (3) rows, each 2.7 metres diameter, along the landside of the Alexander Dock.

Rockfill columns to be constructed at approximate elevation 226 metres and keyed a minimum of 0.3 metres into the underlying till.

4. Construct thirty-four (34) rockfill columns in one (1) row, each 2.7 metres diameter, along the riverside of the Alexander Dock. Works include construction of a temporary construction access platform to facilitate construction of the rockfill columns.

Rockfill columns to be constructed at approximate elevation 222 metres and keyed a minimum of 0.3 metres into the underlying till.

and

Terms and Conditions as set out in Schedule "A" which is attached hereto and forms part of Waterway Permit No. 4/2002-A.

SCHEDULE "A"

(Attached to and forms part of Waterway Permit No. 4/2002-A)

SUBJECT CONTINUED...

Downstream of Alexander Dock to May Street

1. Deposit approximately 3,000 cubic metres of rockfill to a maximum depth of approximately 0.6 metres as an erosion control blanket along approximately 250 lineal metres of shoreline.

Rockfill to be deposited from the OHWM and extend approximately 10 metres beyond winter ice level and blend into existing shoreline downstream of May Street at the Red River.

2. Construct fifty-seven (57) rockfill columns in one (1) row, each 2.7 metres diameter, along approximately 250 lineal metres of shoreline between the downstream end of Alexander Dock to May Street.

Rockfill columns to be constructed along lower bank area at approximate elevation 226 metres and keyed a minimum of 0.3 metres into the underlying glacial till.

3. Construct a granular shear key along approximately 250 lineal metres of shoreline on the riverside of the proposed rockfill columns at regulated summer water level. Shear key construction includes the excavation of a 5-metre wide trench to a minimum depth of 0.3 metres into the basal till and infill of same with approximately 7,200 cubic metres of compacted rockfill.

Galt Avenue Outfall

Perform the following replacement works for construction of a new land drainage outfall at Galt Avenue (RR-64) outfall:

1. Install approximately seventy (70) metres of 1600 mm diameter C.S.P. land drainage sewer outfall pipe complete with barrier screen and end section. Outfall to be installed by open cut method.

All of the above as per drawings and Tender No. 904-2001 "Waterfront Drive (Lombard Avenue to Heaton Avenue) Phase 2 - Riverbank Remediation Works" submitted and date stamped December 31, 2001, including revised KGS Group plans as per Proposed Change Notice No. 1 submitted and date stamped February 4, 2002.

Engineer's report by KGS Group "Waterfront Drive Riverbank Stability Report for Phase II Construction, Riverbank Protection Works, Lombard Avenue to May Street" dated December 31, 2001, including supplemental letter report dated February 1, 2002 submitted in support of application.

CONDITIONS CONTINUED...

3. Subject to City of Winnipeg Tender No. 904-2001 "Waterfront Drive (Lombard Avenue to Heaton Avenue) Phase 2 - Riverbank Remediation Works."
4. The works are to be performed under full-time supervision of the KGS Group hereafter referred to as the Engineer.

(see over)

Schedule "A" continued...

(Attached to and forms part of Waterway Permit No. 4/2002-A)

CONDITIONS CONTINUED...

5. The riverbank is to be monitored (visual/instrumentation) during all phases of construction with Waterways apprised of the results and interpretation of same on a regular basis.
6. Construction activity is to be confined only to the subject work area.
7. Lower bank works are to be carried out during the winter months when the ground is frozen and can sufficiently support vehicular traffic and construction activity.
8. Storage or stockpiling of materials or construction supplies is not allowed on or near the riverbank. (Note: Any storage or stockpile areas are to be approved by the Engineer and located in areas so as not to adversely impact on bank stability.)
9. All excavated material from earthwork operations (rockfill column excavation, trench shear key excavation, etc.) is to be removed directly from the site and not deposited within the regulated area under Waterway By-law No. 5888/92.
10. Any riverbank area damaged as a result of construction or vehicular access is to be restored to a condition at least equivalent to that which existed prior to construction.
11. As the methodology and sequencing of the proposed riverbank stabilization works are very important, the Contractor is to exercise due diligence in ensuring that the implementation process and controls as reviewed and approved by the Engineer are adhered.
12. Rockfill column construction between the Alexander Dock and May Street is to be completed prior to initiation of shear key construction works at regulated summer river level.
13. Under no circumstances shall the Contractor be allowed to leave any excavation open overnight. Key trench excavation shall be done concurrently with backfill/compaction operations so as there are no more than 2 metres of exposed length of excavation along the bottom at any time.
14. The works are to proceed in a timely manner so as temporary stockpiling operations are minimized.
15. The lines and grades of the proposed rockfill riprap are to provide a uniform shoreline and are to be tapered or feathered to smoothly blend with bank contours of adjacent properties so as not to adversely impact on the erosion or stability of the adjacent sections of riverbank.
16. Upon completion of the subject works, the Engineer is to submit as-built drawings including a letter to the Waterways Section, Unit 15, 30 Fort Street, Winnipeg, Manitoba R3C 4X5 certifying that all works have been completed to his satisfaction and in compliance with this Permit.

NOTE: This Permit is issued only for the items identified in the subject. Waterways approval will be required for any supplemental works proposed in the regulated area of Waterways including Phase III upland road works between James Avenue and Heaton Avenue.

(continued...)

Schedule "A" continued...

(Attached to and forms part of Waterway Permit No. 4/2002-A)

CONDITIONS CONTINUED...

NOTE:

This Permit is also issued with the understanding and in the context that the subject stabilization works will be completed prior to the 2002 spring freshet. If for some reason the approved works cannot be completed within this time period, the Engineer must submit a report to the satisfaction of Waterways which addresses the following:

1. Substantiation of the reason(s) that the works cannot be completed.
2. Proposed scheduling and implementation of outstanding works including confirmation of a demonstrated commitment by the Public Works Department, City of Winnipeg to have said works completed within this schedule pursuant to Waterfront Drive (Lombard Avenue to Heaton Avenue) Phase 2 - Riverbank Remediation Works.
3. Measures required to be in place on an interim basis from a geotechnical perspective to ensure public safety, particularly in and around the Alexander Dock.

**The City of Winnipeg
Waterway Permit**

PERMIT NO: 27/2002

Under and by virtue of the powers vested in it under the provisions of The City of Winnipeg Act, The City of Winnipeg hereby authorizes (subject to the conditions hereinafter specified):

APPLICANT:

KGS Group, Attention: Roy Houston, P. Eng., 3rd Floor, 865 Waverley Street, Winnipeg, Manitoba R3T 5P4 APPLICANT CONTINUED...
to undertake work on the following described lands, contained within the regulated area of The City of Winnipeg Waterway By-law namely:

LEGAL DESCRIPTION:

Plan 4776

MUNICIPAL LOCATION:

and commonly known as:

Waterfront Drive (James Avenue to May Street)

SUBJECT:

the following work namely:

Install underground services including renewal works of the City's existing sewer and watermain system as part of Phase 3 construction for Waterfront Drive adjacent the west bank of the Red River between James Avenue and May Street.

All of the above as per Tender Package and Drawings of The City of Winnipeg Tender No. 175-2002 "Watermain Renewal and Land Drainage Sewer Installation for Waterfront Drive Phase 3" submitted and date stamped March 22, 2002.

CONDITIONS:

SUBJECT CONTINUED...

1. Subject to Tender Package and Drawings of The City of Winnipeg Tender No. 175-2002 "Watermain Renewal and Land Drainage Sewer Installation for Waterfront Drive Phase 3".
2. Subject to the Engineer's report prepared by KGS Group, dated April 9, 2002.
3. Any temporary storage or stockpile area(s) required for materials or construction supplies is to be approved by the Contract Administrator and located in area(s) that would not have adverse impact on riverbank stability.

EXPIRATION:

This permit shall expire and the right of the owner under the permit shall terminate if the work authorized by the permit is not commenced within one (1) year from the date of issuance of the permit and reasonably continued without interruption thereafter.

IMPORTANT NOTICE:

The permit holder is reminded that this permit is issued in pursuance of The City of Winnipeg Waterway By-law only, and does not constitute authority for any particular use of the land designated therein which may be within the regulatory powers of the Government of Canada, the Government of Manitoba, or The City of Winnipeg within which the land is situate.

DATED in The City of Winnipeg, in The Province of Manitoba this 18 day of April A.D. 2002

Estimated Cost: \$1,000,000.00
Permit Fee : \$2,099.90
Receipt Number: 0 (Paid JE)



Designated Employee, The City of Winnipeg

APPLICANT CONTINUED...

(on behalf of the City of Winnipeg, Public Works Department,
106-1155 Pacific Avenue, Winnipeg, Manitoba R3E 3P1)

SUBJECT CONTINUED...

Engineer's letter report by KGS Group dated April 9, 2002
submitted in support of application.

The proposed underground servicing works will be located
within the proposed Waterfront Drive street right-of-way,
at closest, approximately 51 metres from the Red River.

APPLICANT CONTINUED...

(on behalf of City of Winnipeg, Public Works Department,
Attn: Fred Corey, P. Eng., Unit 106 - 1155 Pacific Avenue,
Winnipeg, Manitoba R3E 3P1)

SUBJECT CONTINUED...

All of the above as per Roadwork Construction Drawings P-3213-1 to P-3213-12 of City of Winnipeg Tender No. 253-2002 "Waterfront Drive, Phase 4 Roadway Construction from James Avenue to Higgins Avenue" including Addendum No. 1 prepared by KGS Group for the City of Winnipeg, Public Works Department submitted and date stamped July 4, 2002.

Engineer's letter report by KGS Group "Waterfront Drive, Riverbank Stability Assessment of Phase IV Roadworks Construction City of Winnipeg Tender No. 253-2002" dated July 12, 2002 submitted in support of application.

The proposed roadway will be located, at closest, approximately 55 metres from the Red River.

CONDITIONS CONTINUED...

Note:

This permit is issued only in the context of the identified subject works, namely, construction of roadway only. Please be advised that a request to amend this permit to include Phase IV Landscape Development for Waterfront Drive and Stephen Juba Park will be required.



Planning, Property & Development Department
Service de l'urbanisme, des biens et de l'aménagement
 Unit 15 – 30 Fort Street • 30, rue Fort, unité 15 • Winnipeg • Manitoba R3C 4X5
WATERWAY PERMIT

PERMIT NO: 114/2003

Under and by virtue of the powers vested in it under the provisions of The City of Winnipeg Charter Act, The City of Winnipeg hereby authorizes (subject to the conditions hereinafter specified):

APPLICANT: KGS Group
 3rd Floor, 865 Waverley Street
 Winnipeg, Manitoba R3T 5P4
 (on behalf of City of Winnipeg, Public Works Department, 106 – 1155 Pacific Avenue, Winnipeg, Manitoba R3E 3P1)

to undertake work on the following described lands, contained within the regulated area of The City of Winnipeg Waterway By-law namely:

LEGAL DESCRIPTION: Lot 21, Plan 31732; Lot 1 to 3, Plan 27051; Lot 24 to 26, Plan 33032; Part R.L. 8 & 9 St. John; Part Lot 26 to 32, Plan 223; Lot 24 to 27, Plan 32971

MUNICIPAL LOCATION: and commonly known as:
 Waterfront Drive

SUBJECT: the following work namely:
 Carry out landscape development works on the west bank of the Red River on the riverside of Waterfront Drive between Lombard Avenue and Higgins Avenue.
 Works include construction of toboggan slide canopy, paving stone areas, planting beds, landscape timber retaining walls, grading, tree and shrub plantings, benches and installation of pedestrian lighting.
 All of the above as per Tender No. 293-2003 "Waterfront Drive Landscape Development" and Tender No. 308-2003 "Waterfront Drive and Stephen Juba Park: Pedestrian Lighting Installation" submitted and date stamped August 28, 2003.
 Engineer's letter report by KGS Group "Waterfront Drive – Phase 5 Landscape Development and Pedestrian Lighting, Waterway Construction Permit" dated July 7, 2003 submitted in support of application.

CONDITIONS:

1. Subject to the Engineer's letter report by KGS Group "Waterfront Drive – Phase 5 Landscape Development and Pedestrian Lighting, Waterway Construction Permit" dated July 7, 2003.
2. Subject to City of Winnipeg Tender No. 293-2003 "Waterfront Drive Landscape Development".

EXPIRATION: This permit shall expire and the right of the owner under the permit shall terminate if the work authorized by the permit is not commenced within one (1) year from the date of issuance of the permit and reasonably continued without interruption thereafter.

IMPORTANT NOTICE: The permit holder is reminded that this permit is issued in pursuance of The City of Winnipeg Waterway By-law only, and does not constitute authority for any particular use of the land designated therein which may be within the regulatory powers of the Government of Canada, the Government of Manitoba, or The City of Winnipeg within which the land is situate.

Dated in The City of Winnipeg, in the Province of Manitoba this 9th day of September, 2003 A.D.

Estimated Cost: \$650,000.00
 Permit Fee: \$1,424.40
 Receipt Number: JE

 Designated Employee, The City of Winnipeg



Planning, Property & Development Department
Service de l'urbanisme, des biens et de l'aménagement
Unit 15 - 30 Fort Street • 30, rue Fort, unité 15 • Winnipeg • Manitoba R3C 4X5

WATERWAY PERMIT

PERMIT NO: 35/2004

Under and by virtue of the powers vested in it under the provisions of The City of Winnipeg Charter Act, The City of Winnipeg hereby authorizes (subject to the conditions hereinafter specified):

APPLICANT: KGS Group
3rd Floor, 865 Waverley Street
Winnipeg, Manitoba R3T 5P4
(on behalf of City of Winnipeg, owner, c/o Planning, Property and Development Department, Real Estate Division, 2nd Floor, Winnipeg, Manitoba R3C 4K4)

to undertake work on the following described lands, contained within the regulated area of The City of Winnipeg Waterway By-law namely:

LEGAL DESCRIPTION: Lot 8 - 9, D.G.S. 9/10 St. John, Plan 33032

MUNICIPAL LOCATION: and commonly known as:

Alexander Dock - South Half

SUBJECT: the following work namely:

Perform the following structural rehabilitation works to approximately 225 lineal feet of the south portion of Alexander Dock pursuant to City of Winnipeg Tender No. 111-2004.

Works include the demolition of the existing deck, excavation and off-site removal of approximately 450 cubic yards of riverbank material under deck, structural reinforcement of substructure supports including the installation of driven timber end-bearing piles and reconstruction of timber deck.

All of the above as per City of Winnipeg Tender No. 111-2004 "Alexander Dock - South Half Dock Rehabilitation" including Addendum No. 1 and correspondence submitted and date stamped March 22, 2004.

Engineer's letter report by KGS Group "Alexander Dock - South Half Dock Rehabilitation, City of Winnipeg Tender No. 111-2004" dated March 18, 2004 submitted in support of application.

CONDITIONS:

1. Subject to City of Winnipeg Tender No. 111-2004 "Alexander Dock - South Half Dock Rehabilitation" including Addendum No. 1.
2. Subject to the Engineer's letter report by KGS Group "Alexander Dock - South Half Dock Rehabilitation, City of Winnipeg Tender No. 111-2004" dated March 18, 2004.
3. The subject works, in particular under deck regrading and piling operations, are to be performed under Engineering supervision hereafter referred to as the Engineer.
4. Storage or stockpiling of materials or construction supplies is not permitted on or near the riverbank. (Note: Any storage or stockpile areas are to be located as designated by the Engineer so as there is no adverse impact on bank stability conditions.)
5. Excavated soil from all earthwork operations (riverbank excavation, etc.) is to be removed directly from the site and not deposited within the regulated area under Waterway By-law No. 5888/92.
6. Any bank area damaged as a result of access or construction activity is to be restored to a condition, at minimum, equivalent to that which existed prior to construction.

EXPIRATION: This permit shall expire and the right of the owner under the permit shall terminate if the work authorized by the permit is not commenced within one (1) year from the date of issuance of the permit and reasonably continued without interruption thereafter.

IMPORTANT NOTICE: The permit holder is reminded that this permit is issued in pursuance of The City of Winnipeg Waterway By-law only, and does not constitute authority for any particular use of the land designated therein which may be within the regulatory powers of the Government of Canada, the Government of Manitoba, or The City of Winnipeg within which the land is situate.

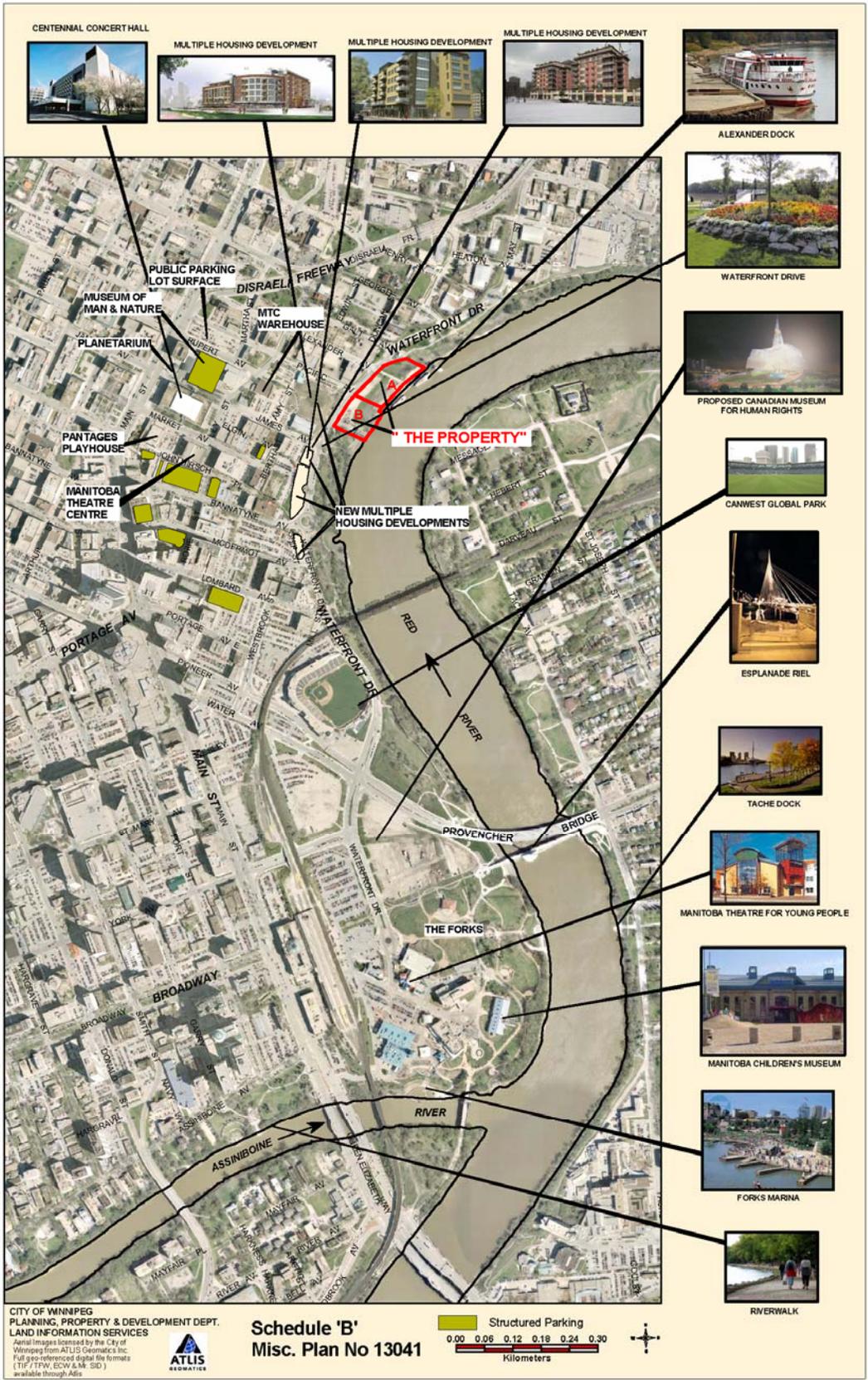
Dated in The City of Winnipeg, in the Province of Manitoba this 25th day of March, 2004 A.D.

Estimated Cost: \$330,000.00
Permit Fee: \$762.60
Receipt Number: JE


Designated Employee, The City of Winnipeg

Schedule "E"

Downtown Context Misc. Plan No. 13041



Schedule "F"

Downtown Zoning By-law 100/2004

Part 6: Riverbank Sector

Intent

600 This sector is intended primarily for the use and enjoyment of the public. Uses supportive of and accessory to a continuous linear parkway and public gathering nodes are encouraged. In order to ensure that views and access to and from the Red and Assiniboine Rivers are enhanced, urban design review in this sector will encourage modestly-scaled buildings and structures, as well as high-quality built form.

Land Use

610(1) The following Land Use regulations are specific to the Riverbank Sector. No new use or expansion of an existing use may be established except in compliance with the following regulations and with the Land Use portion (section 200) of the General Provisions.

610(2) Use Categories:

USE CATEGORIES AND USES		"P" – permitted use "C" – conditional use "A" – accessory use "-" – not permitted use
RESIDENTIAL & RESIDENTIAL-RELATED		
Dwelling		-
Care home		-
Neighbourhood care home		-
Dormitory		-
Single-room occupancy		-
Bed & breakfast		-
Live-work unit		-
COMMERCIAL SALES & SERVICE		
Retail sales		A to Park uses
Wholesale sales		-
Personal services		A to Park uses
Business support services		-
Medical/dental/optical/counselling clinic		-
Veterinary, not including kennel		-
Bank/financial service		-
Private club, not licensed		-
Hostel		-
Hotel		-
Restaurant		A to Park uses

Downtown Winnipeg Zoning By-law No. 100/2004

COMMERCIAL SALES & SERVICE (cont'd.)	
Drinking establishment	-
Studio for artists/dancers/designers/musicians/ photographers, including production from finished materials	A to Park uses
Billiard hall, participant	-
OFFICE	
Office	A to Park uses
Call centre	-
PRIVATE MOTOR VEHICLE-RELATED	
Auto/light truck/motorcycle rental & sales, entirely within enclosed building	-
Auto/light truck/motorcycle repair & service, entirely within enclosed building	-
Fuel sales	-
Drive-in, Drive-through	-
Parking, non-accessory offstreet at grade	-
Parking, non-accessory offstreet above/below grade	-
TRANSPORTATION, UTILITY, & COMMUNICATIONS	
Power station, electrical/heating/cooling/ communications distribution facility	-
Bus depot	-
Train station	-
Commercial marina	C
Radio/television broadcast tower	-
Wireless communications tower	-
Loading, non-accessory	-
PUBLIC & INSTITUTIONAL	
Church/religious institution	-
School	-
Daycare	-
Community/recreation centre, primarily participant	P
Courthouse	-
Jail/detention centre	-

Downtown Winnipeg Zoning By-law No. 100/2004

PUBLIC & INSTITUTIONAL <i>(cont'd.)</i>	
Neighbourhood rehabilitation home	-
Emergency residential shelter	-
Library	-
Hospital, including overnight	-
Protection & emergency services	-
Research institution	-
CULTURAL & ENTERTAINMENT	
Auditorium/concert hall/theatre/cinema	C
Convention centre/exhibition hall	-
Amusement enterprise	-
Casino	-
Cultural centre	-
Gallery, primarily spectator	A to Park uses
Museum	A to Park uses
Sports & entertainment arena/stadium, primarily spectator	-
Studio, radio/TV/motion picture broadcast & production	-
Dance hall	-
Banquet hall	-
PARK & PARK-RELATED	
Park/plaza/square/playground, public	P
Trail/walkway, public	P
Boat dock, public	P
LIGHT INDUSTRIAL	
Manufacture/assembly/repair/packaging of products from prepared materials, entirely within enclosed building	-
Warehouse, entirely within enclosed building	-
Printing/publishing	-
Commercial laundry/dry-cleaning plant	-
Brewery/distillery/winery	-
Commercial bakery	-
Recycling centre	-

Downtown Winnipeg Zoning By-law No. 100/2004

RESTRICTED	
Adult entertainment	-
X-rated store	-
Dating & escort service	-
Massage parlour	-
Pawnshop	-
Halfway house/residential treatment centre	-
ACCESSORY	
Parking, to all uses offstreet at-grade not exceeding greater of 10 parking spaces or 40% of lot area; to all uses above- or below- grade, no restrictions	A
Loading, to all uses not exceeding 20ft wide X 30ft long, unless located entirely within building	A
Refuse storage, to all uses	A
Sign, to all uses	A
Retail sales, to Park	A
Personal services, to Park	A
Restaurant, to Park	
Studio for artists/dancers/designers/musicians/ photographers, including production from prepared materials, to Park	A
Office, to Park	A
School, to Park	A
Research institution, to Park	A
Gallery, primarily spectator, to Park	A
Museum, to Park	A
Home occupation, to Dwelling	-
Parish hall, to Church	-
Micro-brewery/winery/distillery, to Restaurant	-
Bakery, to Restaurant and Grocery	-

Bulk

620(1) The following Bulk regulations and guidelines are specific to the Riverbank Sector. No new building or structure, or expansion of an existing structure, may be developed or enlarged within this sector that does not comply with the following regulations and the Bulk portion (section 210) of the General Provisions. Subsection 620(2) provides for basic Bulk regulations for this sector.

Downtown Winnipeg Zoning By-law No. 100/2004

620(2) Bulk regulations:

RIVERBANK SECTOR	BASIC
Building Height Maximum	4 storey
Floor Area Ratio Maximum	2
Front, Side, Corner Side Yards	Not applicable
Residential Uses Interior Side Yards Minimum*	1- to 4-storey: 10 feet
	5-storey and above: 20 feet
Residential Uses Rear Yards Minimum**	20 feet
<p>* - no interior side yard is required when no doors, windows, or other openings are provided in the wall facing the adjacent property and in the adjacent property wall (see section 210)</p> <p>** - 1-storey to 4-storey 20 foot minimum rear yard may be calculated from the building wall to the mid-point of an existing public road or lane (see section 210)</p>	

620(3) Bulk Guidelines

Signs

630(1) The following Sign regulations are specific to the Riverbank Sector. No new sign or sign structure and no alteration to an existing sign or sign structure may be established or erected except in accordance with the following regulations and the Signs portion (section 220) of the General Provisions. Subsection 630(2) provides basic Sign regulations for this sector.

Downtown Winnipeg Zoning By-law No. 100/2004

630(2) Sign regulations:

(a)

USE IDENTIFICATION TYPE		BASIC
Total Sign Volume		2 square feet per frontage foot*
Sign Form	Attached	All
	Free-standing	None
Sign Size Maximum		32 square feet
Sign Structure Height Maximum, Free-standing		Not applicable
Sign Location, Attached		1- and 2-storey building: below parapet
		3-storey and above building: below 2nd storey ceiling
* - where property does not have frontage on a public road, 200 square foot total per building.		

(b)

BUILDING IDENTIFICATION TYPE		BASIC
Total Sign Volume		1.5% of building wall*
Sign Form	Attached	Fascia, awning, canopy
	Free-standing	None <i>roof</i>
Sign Size Maximum		Not applicable
Sign Structure Height Maximum, Free-standing <i>roof</i>		Not applicable <i>10 above parapet</i>
Sign Location	Attached	1 sign per building wall maximum**
	Free-standing	Not applicable
* - 48 square foot maximum sign size, when 1.5% of building wall calculation results in less than 48 square feet		
** - does not apply to those signs located below 2nd storey ceiling.		

Downtown Winnipeg Zoning By-law No. 100/2004

(c)

EVENT TYPE*		BASIC
Total Sign Volume		Not applicable
Sign Form	Attached	Fascia, projecting, marquee
	Free-standing	Podium, ground-oriented
Sign Size Maximum		48 square feet
Sign Structure Height Maximum, Free-standing		10 feet
Sign Location	Attached	1- and 2-storey building: below parapet
		3-storey and above building: below 2nd storey ceiling
	Free-standing	1 sign per property maximum
* - can only be established in conjunction with "Cultural & Entertainment" and "Public & Institutional" uses, restaurants, and drinking establishments		

(d)

DIRECTIONAL TYPE		BASIC
Total Sign Volume		Not applicable
Sign Form	Attached	All
	Free-standing	All
Sign Size Maximum		4 square feet per sign
Sign Structure Height Maximum, Free-standing		10 feet
Sign Location	Attached	1- and 2-storey building: below parapet
		3-storey and above building: below 2nd storey ceiling
	Free-standing	Not applicable

Downtown Winnipeg Zoning By-law No. 100/2004

(e)

PARKING INSTRUCTIONS TYPE		BASIC
Total Sign Volume		Not applicable
Sign Form	Attached	All
	Free-standing	All
Sign Size Maximum		24 square feet per sign
Sign Structure Height Maximum, Free-standing		10 feet
Sign Location	Attached	1- and 2-storey building: below parapet
		3-storey and above building: below 2nd storey ceiling
	Free-standing	Not applicable

Parking

640(1) See Parking portion (section 230) of the General Provisions. In addition, refer to section 610 regarding Parking as a Land Use in the Riverbank Sector.

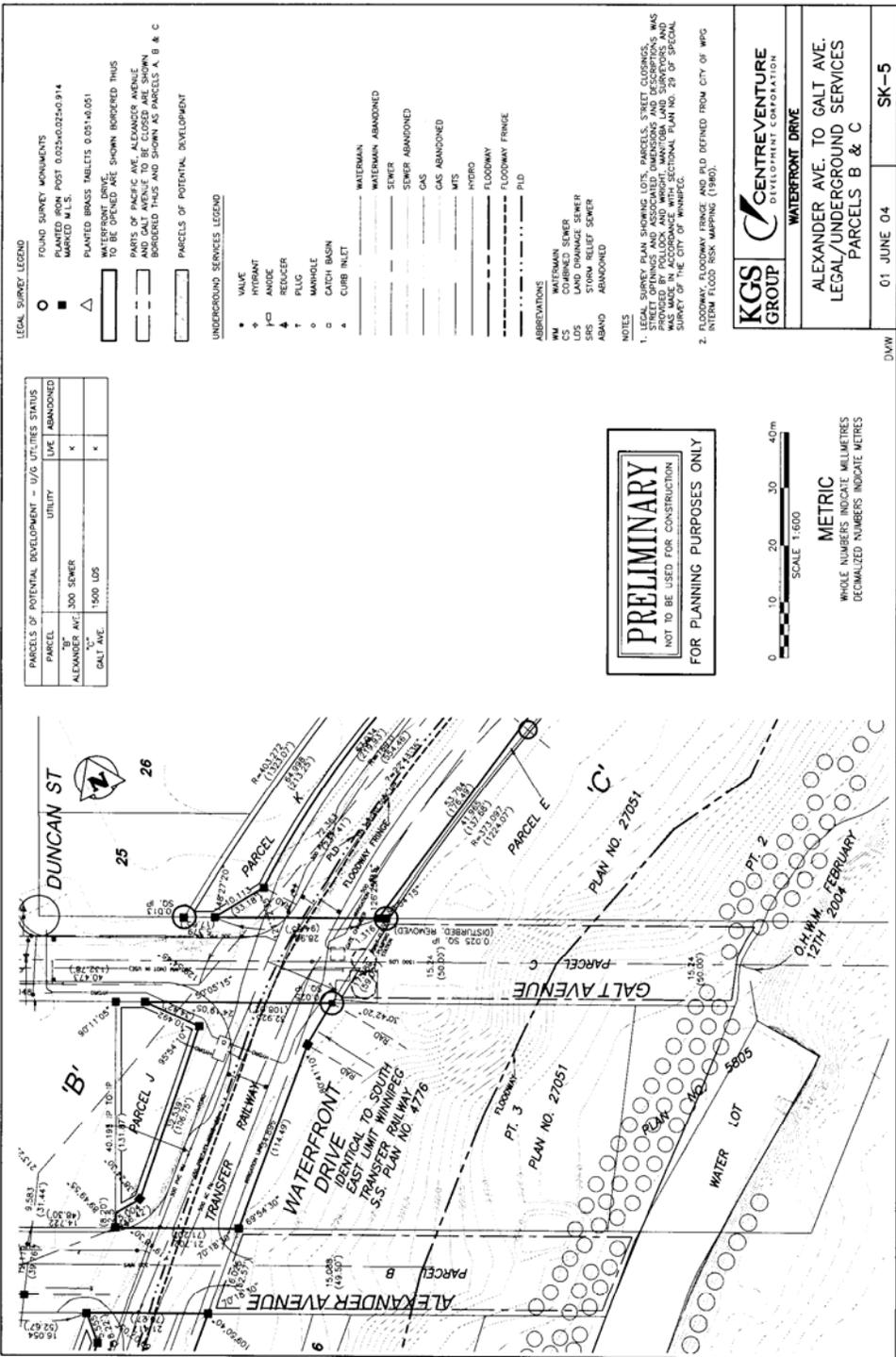
Loading

650(1) See Loading portion (section 240) of the General Provisions. In addition, refer to section 610 regarding Loading as a Land Use in the Riverbank Sector.

Urban Design

660(1) See the Urban Design portion (section 250) of the General Provisions. In addition, refer to *The Forks National Historic Site* map in Appendices.

Schedule "G"
Sub-surface Utilities



PARCELS OF POTENTIAL DEVELOPMENT - U/G UTILITIES STATUS

PARCEL	UTILITY	LIVE	ABANDONED
ALC	300 SEWER	x	
ALD	1500 LGS		x

- LEGAL SURVEY LEGEND**
- FOUND SURVEY MONUMENTS
 - PLANTED IRON POST 0.025x0.025x0.914 MARKED M.L.S.
 - PLANTED BRASS TABLETS 0.051x0.051
 - WATERFRONT DRIVE TO BE OPENED ARE SHOWN BORDERED THUS
 - PARTS OF PACIFIC AVE, ALEXANDER AVENUE AND GALT AVENUE TO BE CLOSED ARE SHOWN BORDERED THUS AND SHOWN AS PARCELS A, B & C
 - PARCELS OF POTENTIAL DEVELOPMENT

- UNDERGROUND SERVICES LEGEND**
- WATER
 - SEWER
 - SEWER ABANDONED
 - GAS
 - GAS ABANDONED
 - HYDRO
 - FLOODWAY
 - FLOODWAY FRINGE
 - PLD
- ABBREVIATIONS**
- WM WATERMAN
 - CS COMBINED SEWER
 - LDS LAND DRAINAGE SEWER
 - SRS STORM RELIEF SEWER
 - ASND ABANDONED
- NOTES**
- LEGAL SURVEY PLAN SHOWING LOTS, PARCELS, STREET CLOSURES, STREET OPENINGS AND ASSOCIATED DIMENSIONS AND DESCRIPTIONS WAS MADE IN ACCORDANCE WITH SECTIONAL PLAN NO. 29 OF SPECIAL SURVEY OF THE CITY OF WINNIPEG.
 - FLOODWAY, FLOODWAY FRINGE AND PLD DEFINED FROM CITY OF WPG INTERM FLOOD RISK MAPPING (1980).

PRELIMINARY
NOT TO BE USED FOR CONSTRUCTION
FOR PLANNING PURPOSES ONLY



METRIC
WHOLE NUMBERS INDICATE MILLIMETRES
DECIMALIZED NUMBERS INDICATE METRES

KGS GROUP

CENTREVENTURE DEVELOPMENT CORPORATION

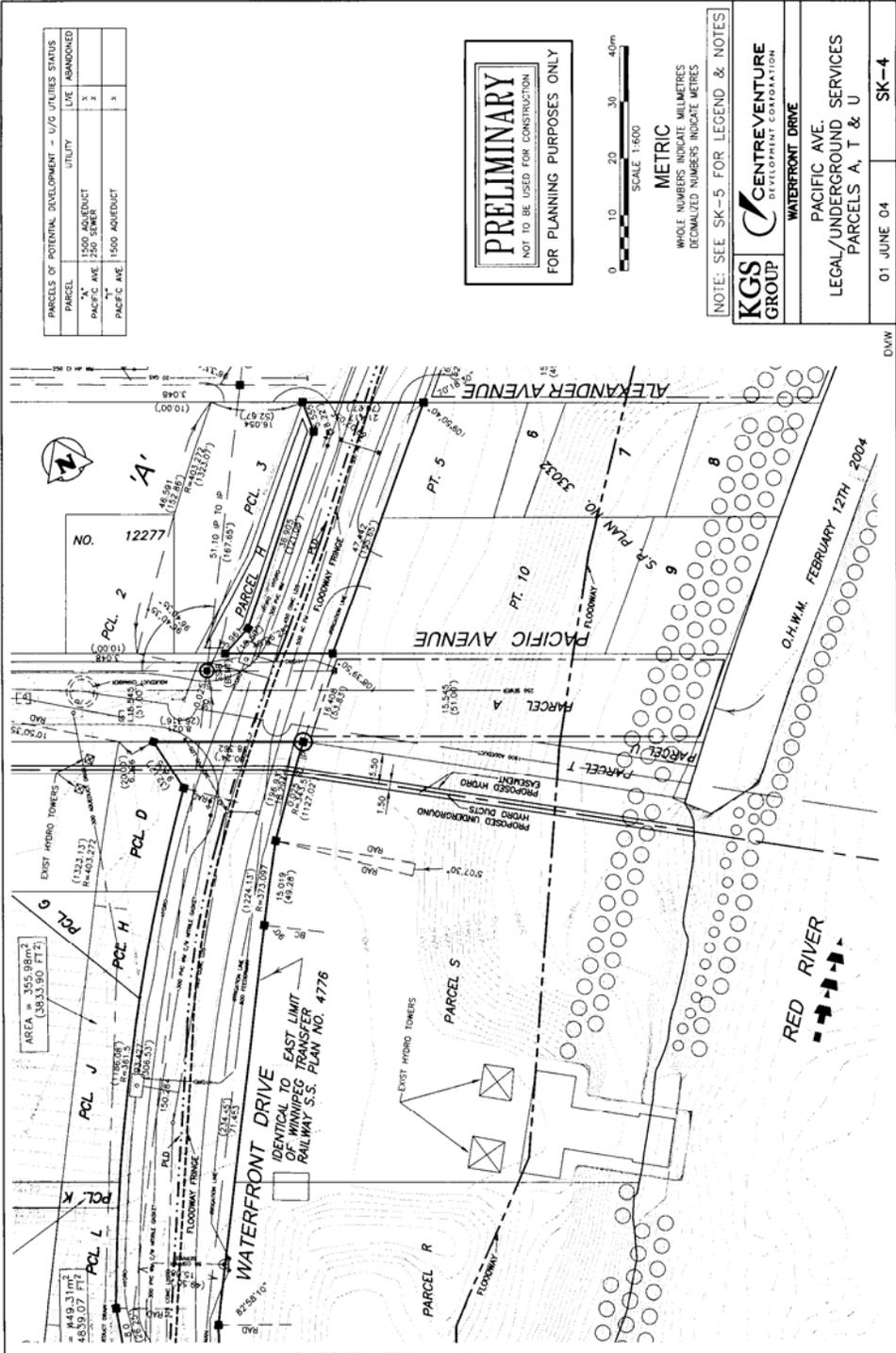
WATERFRONT DRIVE

ALEXANDER AVE. TO GALT AVE. LEGAL/UNDERGROUND SERVICES PARCELS B & C

01 JUNE 04

SK-5

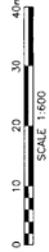
DWM



PARCELS OF POTENTIAL DEVELOPMENT - 1/4 JUILIES STATUS

PARCEL	UTILITY	LIVE	ABANDONED
"A"	1500 AERODUCT	X	
PACIFIC AVE	250 SEWER		X
PACIFIC AVE	1500 AERODUCT		X

PRELIMINARY
NOT TO BE USED FOR CONSTRUCTION
FOR PLANNING PURPOSES ONLY



METRIC
WHOLE NUMBERS INDICATE MILLIMETRES
DECIMALIZED NUMBERS INDICATE METRES

NOTE: SEE SK-5 FOR LEGEND & NOTES

KGS GROUP
CENTREVENTURE
DEVELOPMENT CORPORATION
WATERFRONT DRIVE

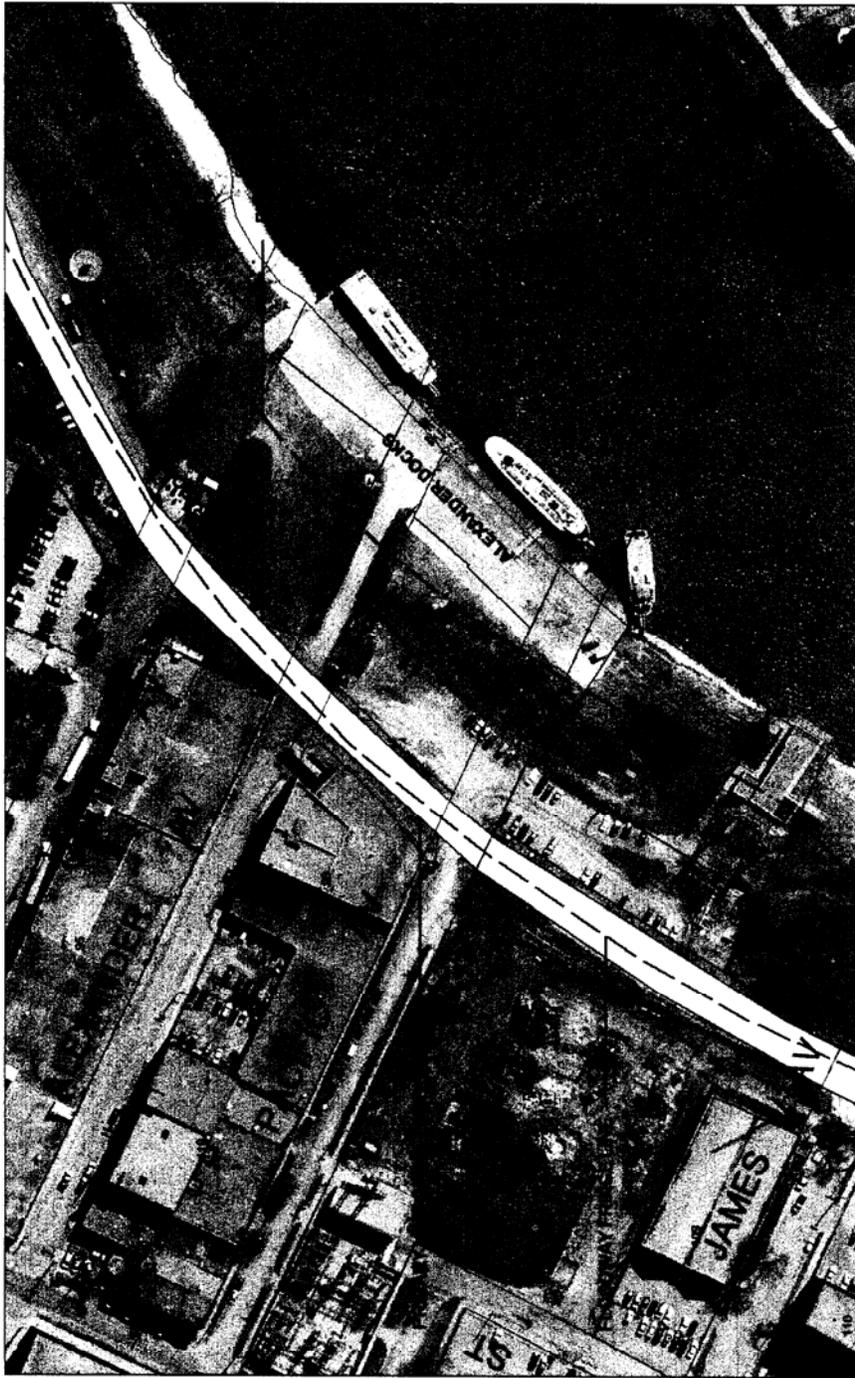
PACIFIC AVE.
LEGAL/UNDERGROUND SERVICES
PARCELS A, T & U

01 JUNE 04
SK-4

DWK

KGS FILE NO. 04-0735-011MAN/DW05/04-0735-01-SK-4-SK-5-DWG
11'x17' (1/01) SCALE: 1:1

Schedule "H"
Floodproofing



Schedule "I"

Eye-level Photographs





View westward across the Red River showing the Alexander Dock



View Westward across the Red River showing the Alexander Dock and Harbourmaster Building



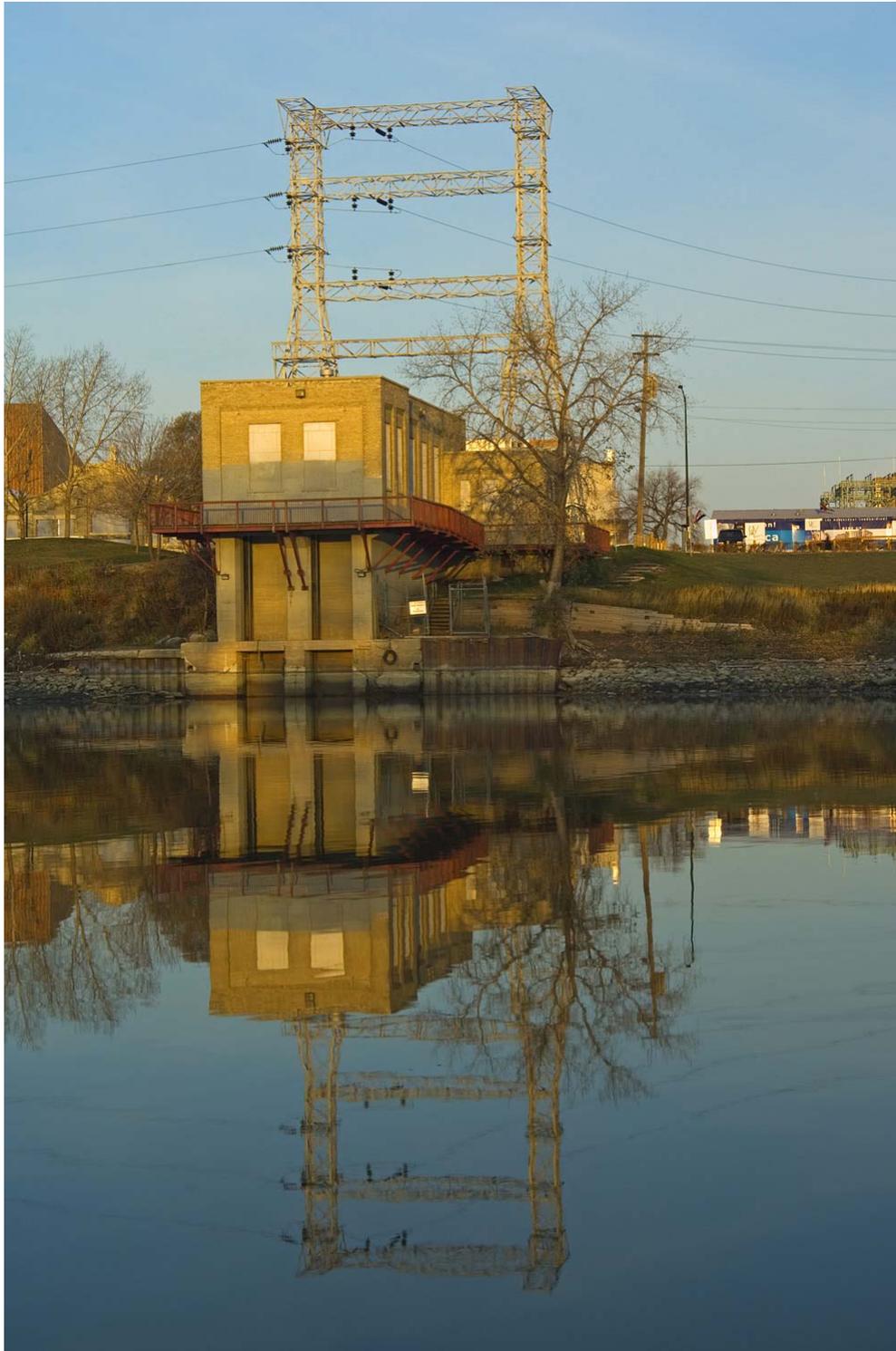
The Northern Façade of Building and view southward



The Southern Façade and view northward



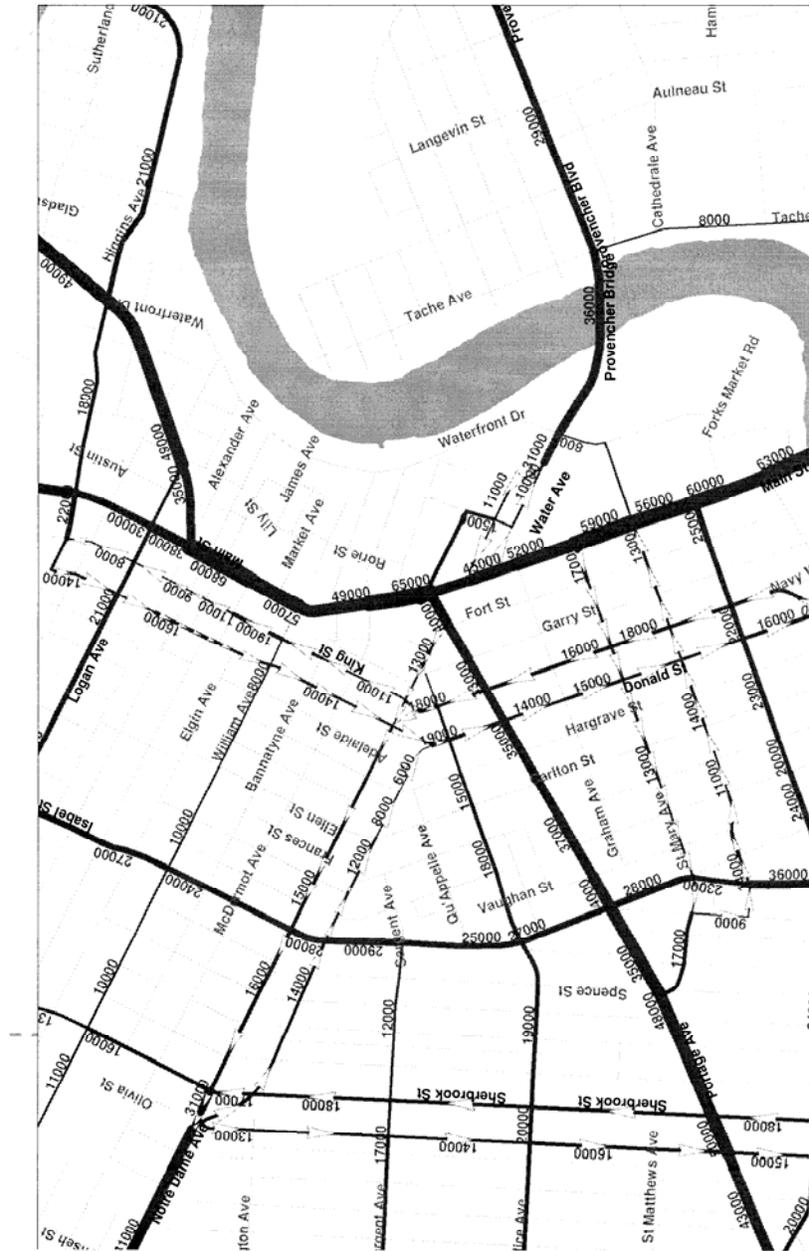
The Western Façade and its view Eastward
Note: The Hydro towers have been removed



The Eastern Façade and its view Westward

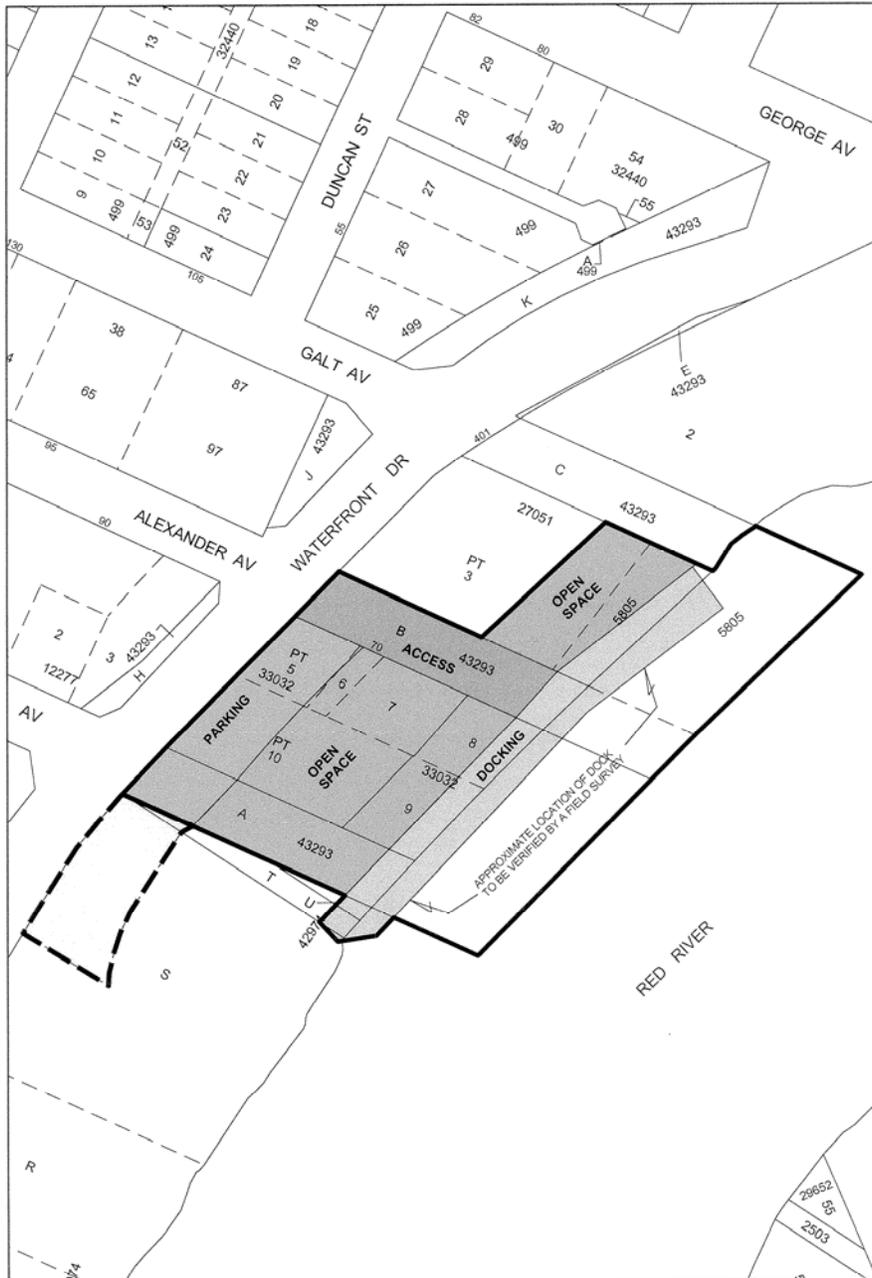
Schedule "J"

Traffic Data



Schedule "K"

Identification of Leased Lands to Paddlewheel Riverboats Ltd.



-  Proposed Lease
-  Alexander Dock Section 2
-  Additional Lease



Date: 2006-APR-28
 District: CC
 File: 320/2
 Scale: 1:1000

METRIC

NOTE:
 Information displayed hereon has been compiled or computed from a variety of sources
 and should be used as a general guide only.
 No warranty is expressed or implied regarding the accuracy of such information.

**CITY OF WINNIPEG
 PLANNING, PROPERTY AND DEVELOPMENT DEPT.
 LAND INFORMATION & MAPPING SERVICES**

MISC. PLAN NO. 7806 / 15